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Appendix D

Stakeholder Meetings



Minutes of Meeting

Date: April 12, 2017
File #: TPB166147
Meeting Date & Time: April 6, 2017
3:00 pm
Meeting at: Town of Oakville Town Hall, Trafalgar Room
Subject: Class Environmental Assessment Study
Lakeshore Road West Improvements
Stakeholder Meeting #1

Attendees:

Syed Rizvi, Town of Oakville	Paul Cotes, Bronte Village BIA
Jill Stephen, Town of Oakville	Marie Descent, Oakville Historical Society
Ralph Robinson, Town of Oakville, Councillor - Ward 1	Susan Wells, Oakville Historical Society
Julie Pennal, Town of Oakville, Sir John Colborne Recreational Centre for Seniors	David Sinke, Amec Foster Wheeler
Don Cox, Coronation Park Residents Association	Neal Smith, Amec Foster Wheeler
Peter Donlevy, Coronation Park Residents Association	Louise McAndrew, Amec Foster Wheeler

Absent:

Shelley Thornborrow, Bronte Village Residents Association	Gabe Charles, Heritage Oakville
Pamela Knight, Coronation Park Residents Association	Paul Baille, Southwest Central Oakville Residents Association
George Chisholm, Oakville Historical Society	Diana Trask, West Harbours Residents Association
Ann Sargent, Bronte Village BIA	Jeremy Hardman, Oakville Cycling
Gary Reed, Bronte Village BIA	Bob Felker, Amec Foster Wheeler
Chris Bishop, Association of Oakville Harbours Stakeholders	Heather Dearlove, Amec Foster Wheeler
Karen Solursh-Smith, Bronte Village Mall	

PLEASE NOTE: If there is any comment or amendment to be made to these meeting notes, they should be brought to the notice of Amec Foster Wheeler within 24 hours of issue and confirmed in writing

MATTERS DISCUSSED

ACTION BY:

Introductions

1. Syed Rizvi welcomed stakeholders, introduced Amec Foster Wheeler and initiated self introductions from attendees including their interest in the project. Note: All attendees were provided with a copy of the presentation and a copy of the PIC #1 Notice.
2. The Oakville Historical Society is interested in the historical aspects of the project area and how to preserve and enhance what is within the site.
3. The Bronte BIA is interested in improving the face of Lakeshore road in the Bronte area and bringing activity through to the east end of the village area.
4. The Coronation Park Residents Association is interested in retaining the characteristics of their neighbourhood.
5. The Colborne Seniors Centre Representative is interested in anything that will affect transportation for seniors along the area and specifically within the area of Third Line, and any potential road closures.
6. Ralph Robinson, the Ward 1 Town Councillor, is interested in anything that will impact his constituents, in particular for the improvement of Bronte Road and Lakeshore Road West intersection.

Project Introduction

7. Amec Foster Wheeler provided a presentation to outline the project and to guide discussion with the following agenda:
 - Introductory Remarks
 - Summary of the Class Environmental Assessment Process
 - Planning and Policy Context
 - Environmental Inventories
 - Transportation and Traffic Study and Road Safety Investigation
 - Key Issues Identified by the Project Team
 - Roundtable Discussion
 - Next Steps
8. Amec Foster Wheeler informed the group that we are looking for a solution that fits environmental and public needs and preserves the historical corridor
9. Amec Foster Wheeler provided constraints/opportunities maps which illustrated issues identified by Amec Foster Wheeler within the corridor and asked the group for additional input if they noticed anything missing.

MATTERS DISCUSSED

ACTION BY:

10. Amec Foster Wheeler requested that the assembled stakeholders stop the presentation as we went along, with any questions they might have.

Presentation

Summary of the Class Environmental Assessment Process

11. This process is a class "C" environmental assessment based on the size, cost, and potential length of the project
12. Currently in Phase 2 of the process – identify and evaluate alternate solutions and identify preferred solution. Will be followed by Phase 3 where more detailed design will take place.
13. Next step in the process is a Public Information Centre (PIC) which will take place on April 20, 2017 at 6pm at the Town Hall, Oakville, South Atrium.

Planning and Policy Context

14. This study is taking place within the context of a variety of previous studies and master plans commissioned by the town. It will also occur concurrently with some ongoing processes including the Oakville Harbours Master Plan.

Environmental Inventories

15. To evaluate the study corridor a number of Environmental Inventories are planned or in process.

Q1: Which Environmental Inventories have been completed to date?

A1: Most Environmental Inventories are currently in draft. Traffic and Safety inventories have been completed but have not been reviewed by the Town. Noise Impact Assessment will not be completed until the footprint of the road is finalized.

Q2: Are Environmental Inventory reports available for public review?

A2: To date all reports are in draft and need to be reviewed by the Town. Town commented that final reports would be uploaded to the website.

16. Overview of the environmental context, both aquatic and terrestrial. Several Species at Risk (SAR) have been identified as being located within the general area but field studies have not yet been undertaken to determine if they are located within the study corridor.

Q3: How will roadwork effect creeks and waterways [water crossings and culverts]?

MATTERS DISCUSSED

ACTION BY:

A3: There are many factors that need to be addressed before a recommendation on crossing will be made. Factors that need to be analyzed:

- Is the bridge up to current standards
- Water crossing design could impact existing structure
- Geomorphic assessment (meandering of the river)
- Protection of fisheries
- 100 yr storm analysis
- Condition of structure, service life

All crossings within the study area will be reviewed because existing structures tend to be inadequate to today's standards and theories. The theory of watercourse design has changed since Lakeshore was first constructed. Part of the process is a Geomorphology study. Structures will be evaluated based on a variety of implications including protecting and enhancing fisheries.

17. There is a variety of stormwater management approaches within the corridor including urban, rural, and mixed. It was noted that the whole corridor needs to be addressed to form a coherent process that meets current stormwater management theory.

18. Stage 1 Archaeological Assessment (AA) has been undertaken with 11% of the study area identified as requiring a Stage 2 AA. Noted that the most likely areas for archaeological significance are adjacent to watercourses. It was also noted that Amec Foster Wheeler will undertake a full walkthrough of the corridor to identify any previously unclassified archaeological resources.

Q4: What is the process that will be followed for the stage 2 AA?

A4: Test pits will be dug in areas identified to have high potential for archaeological significance (previously undisturbed). It is expected that arrowheads and flints may be found along watercourses.

19. It was noted that for the built heritage and cultural landscape assessment the locations listed on the constraints/opportunities map were taken from the Town of Oakville's heritage website page.

Q5: Would some of the heritage resources listed include lampposts etc.?

A5: Amec Foster Wheeler is currently unsure of the answer to this question. More information regarding the range of heritage features within the corridor will be identified within the built heritage study process.

20. Amec Foster Wheeler will connect the Oakville Historical Society members with our Built Heritage consultant.

MATTERS DISCUSSED

ACTION BY:

Transportation and Traffic Study and Road Safety Investigation

21. The traffic study reviewed both existing (2016) and future (2021 and 2031) traffic conditions. It was noted that no need for significant widening was found but that certain intersections and localized areas have traffic concerns. It was also noted that the project goal is to develop smart improvements at specific locations.

Q6: What will be done and how will it improve in the Bronte Village area [specifically the retail environment].?

A6: The traffic study as well as the design will focus the needs of all users. It will take into consideration parking access to retail stores, accessibility and exposure. The design is reviewed by all departments of the Town to ensure the needs of Bronte village are represented. Amec Foster Wheeler will work with the BIA to develop a list of their concerns and issues.

22. Identified the recommended road configuration as a 3-lane cross-section, with one lane in each direction and a centre two-way left turning lane. This should generally fit within the existing footprint of the roadway within the corridor with only minor widening at points.

23. Parking is identified as a key issue. It was noted that existing by-law for on street parking in the Town of Oakville is a 3-hour limit. It will be reviewed if existing parking needs to be expanded upon.

Q7: It was noted that there are a large number of construction projects on the go in the area, including the mall, East St., and Marine Dr. There is concern over the amount of activity limiting parking availability within the study corridor, specifically within Bronte Village.

24. Amec Foster Wheeler noted that within the corridor there are 31 intersections and >100 driveways and accesses. Roadway capacity is lost by blocking due to left turning vehicles and busses stopping to pickup/drop off riders. Addition of centre turning lane will reduce those blockages, creating better traffic flow and capacity. It was also noted that some minor traffic signal time changes could improve traffic flow.

Q8: As we move toward a future that could include driverless cars does this study and related construction mean that there will not need to be additional construction or expansion to the roadway?

A8: Future usage is projected through transportation modelling. There is an assumption that a large shift will be made in terms of transportation methodologies from vehicular to other modes (transit, rideshare, cycle and pedestrian). Also due to the nature of the roadway as a minor arterial roadway and location within the

Continued...

Meeting Date: April 6, 2017

MATTERS DISCUSSED

ACTION BY:

town there is not much more traffic or population expected to be added to the roadway use.

Q9: Does this planning include bike lanes?

A9: It has been recognized that within the corridor the current bike infrastructure is sporadic. Within the Active Transportation Master Plan and this study it will be important to identify areas to improve cycling infrastructure.

Q10: What is the current width of the road allowance?

A10: 26m

25. It was noted that there seem to be a high number of collisions within the study corridor. It was also noted that the data provided by the Town was the most current they had, but may not contain the most current incidents.

Q11: What are the worst intersections within the area?

A11: Specific information was not readily at hand but identified Bronte, Third line and Fourth line as having the highest number of accidents. All types of traffic issues are being taken into account, including pedestrian/ bicycle incidents and fatalities.

26. It was noted that one method to reduce accidents is to manage speeds within the corridor through the use of traffic calming measures.

Key Issues Identified by the Project Team

27. Key issues identified for the study corridor are:

- Potential Corridor Improvements
- Bronte Village
- Drainage and Stormwater Management
- Active Transportation

Q12: Clarification for the process undertaken so far was requested.

A12: It was identified that at this point Amec Foster Wheeler is gathering information. For example, with traffic, we are identifying signal warrants and reviewing safety concerns. As we get further into the process we will continue to refine designs and plans.

Q13: Are there going to be requirements for partnerships with businesses to build pedestrian infrastructure [reference to Amica project]?

A13: Generally, project work is covered by the capital works budget. If not naturally encompassed within the Lakeshore corridor there might be additional costs to stakeholders. Part of the

Continued...

Meeting Date: April 6, 2017

MATTERS DISCUSSED

ACTION BY:

process is to review where people want to cross the road as well as what is warranted from a technical perspective.

28. It was noted that many seniors have difficulty crossing at the intersection of Lakeshore Road and Bronte Road.
29. It was noted that this study is an iterative process where the first PIC identifies needs and the second PIC introduces the detailed design.
30. Sir John Colbourn Seniors Centre was identified as a priority area relating to improving road user safety. It was also noted that there was a cyclist fatality in 2016.

Q14: It was noted that the area around Sir John Colbourn Seniors Centre is:

- Traffic heavy
- The bus stop is across the street from the centre requiring seniors to cross the busy road
- It is hard for seniors to cross the street
- Only the care van goes into the centre parking lot
- There can be a large number of seniors crossing the road
- The length of the crossing signal is not long enough for seniors to cross
- There have been numerous complaints at the Centre regarding access
- School traffic from the nearby Oakville Christian School causes backups around 8am and 3pm

Q15: It was also noted that there is a sight line issue at Third Line due to the curvature of the road. As well, there is a high volume of left turns onto Third line.

31. Appleby College – it was noted that Suffolk street has restricted access. The Town was requested to look into why this was implemented and if the restrictions are still required.
32. Bronte Village is identified as a key area in the corridor.

TOWN

Q16: An interest was noted relating to signage in the Bronte Village BIA area.

A16: Noted there are some signage issues. Also noted, that Town departments are involved in the Class EA and have an interest in the design and features being used in the Bronte Village area.

33. It was noted that Amec Foster Wheeler has a Landscape Architect who will create plans within the corridor especially within the Bronte

Continued...

Meeting Date: April 6, 2017

MATTERS DISCUSSED

ACTION BY:

Village Area. As well, the Town of Oakville has an Urban Design Team.

Q17: What is the timing of this study relating to other master planning that is going on?

A17: This study overlaps with the Harbours Master Plan timeline.

- 34.** It was noted that there is a big concern with trucks towing boats into the harbour. The current turning radius to the harbour entrance does not support these vehicles.

Q18: How connected are the various Town departments relating to this study?

A18: All the related departments have met and will continue to be connected throughout this process. The staff team focuses on a comprehensive big picture approach.

- 35.** In regards to drainage and stormwater management throughout the corridor the objective is to rationalize the drainage system creating a consistent approach throughout.

- 36.** Low Impact Development (LID) will be evaluated in terms of improving natural hydrology and diverting water into the groundwater system. This will allow for natural cleaning and filtering of the water rather than collecting and concentrating contaminants in standard pond systems.

- 37.** Noted goal is to avoid washouts and issues especially around Coronation Park.

- 38.** Active Transportation – It was noted that Strava was used from the town's website to gather information on pedestrian and cycling movements on Lakeshore Road West. Also noted that the Town's Active Transportation Master Plan will be used as a reference document.

- 39.** It was noted that at the Fourteen Creek bridge there is the possibility it will need to be widened slightly to accommodate active transportation infrastructure.

Q19: Are there any issues that may occur relating to the Bronte Harbor Yacht Club Lease.

A19: Unaware of issues that may arise but more information may be required.

- 40.** It was requested to determine if the Town has any record of issues and calls relating to the corridor.

TOWN

MATTERS DISCUSSED

ACTION BY:

Anything that occurred leading up to the study has already been passed on, however, traffic operations may have information directed directly to them. Town will check with traffic operations.

Roundtable Discussion

Q20: What is wrong with the current roadway, and why are we going through this process?

A20: It is important to coordinate and look ahead for the needs of the community. Goal is to incorporate and coordinate with the Active Transportation Master Plan. Localized traffic issues should be addressed. It is also important to address current stormwater ponding issues and pedestrian safety issues.

Q21: What is the timetable for completion of the entire project?

A21: 17 studies are currently underway. Next step is detailed design for the first section which is scheduled for 2018. Depending on budget priorities, construction will take place in 3 stages for 3 to 5 years following. There is a potential for some preliminary work earlier. The current study will progress to the preliminary design point by the end of 2017.

- 41. It was again requested that stakeholders review the constraint/opportunities maps for any issues and to add to them.
- 42. Syed Rizvi thanked everyone for their attendance and input and adjourned the meeting.

Meeting Minutes prepared by,

Amec Foster Wheeler Environment & Infrastructure
A Division of Amec Foster Wheeler Americas Limited

Per: Louise McAndrew, B.Sc., CAPM
Junior Environmental Planner

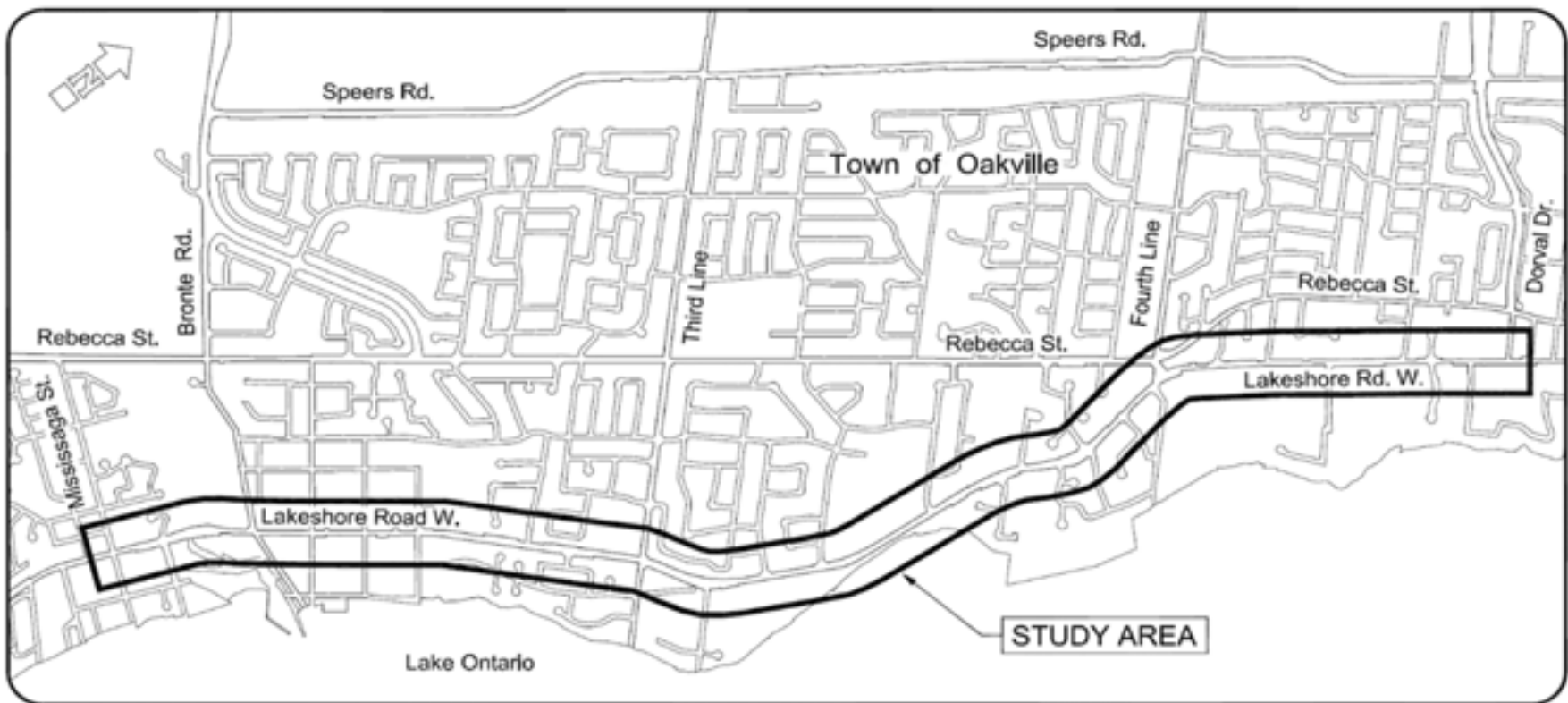
Attachments:

Presentation – Lakeshore Road West Improvements Stakeholder Group Meeting #1
Constraint/Opportunities Maps

Agenda

- 1. Introductory Remarks**
- 2. Summary of the Class Environmental Assessment Process**
- 3. Planning and Policy Context**
- 4. Environmental Inventories**
- 5. Transportation and Traffic Study & Road Safety Investigation**
- 6. Key Issues Identified by the Project Team**
- 7. Roundtable Discussion**
- 8. Next Steps**

Study Area

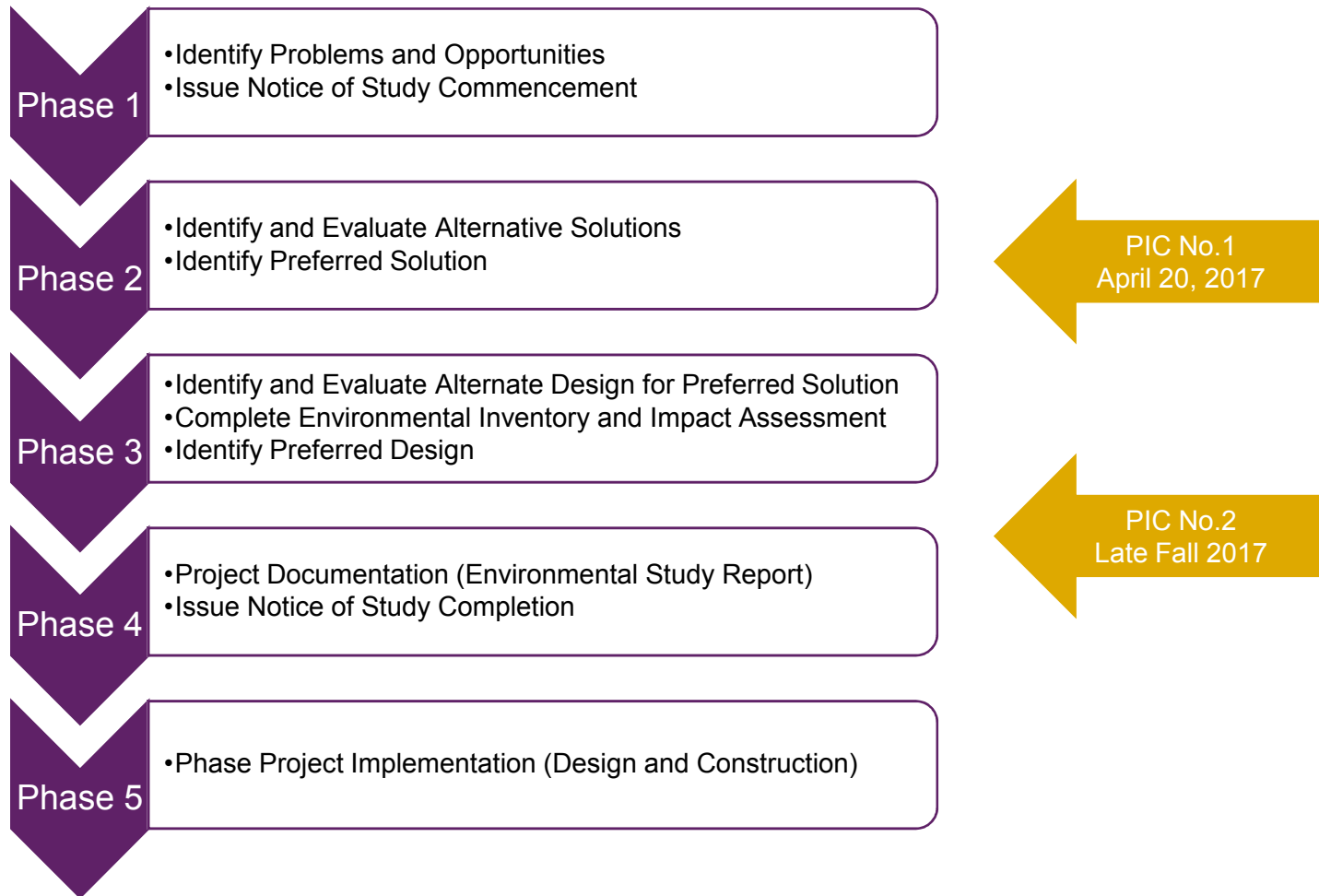


Lakeshore Road West (Mississaga Street to Dorval Drive)





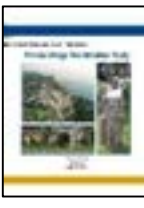



Municipal Class EA Process

- Many projects related to municipal water supply, sewers and transportation systems that are *similar in nature, are carried out routinely, and have predictable and mitigable* environmental effects are investigated according to the Municipal Engineers Association "*Municipal Class Environmental Assessment,*" (October 2000, as amended 2007).
- The Municipal Class Environmental Assessment (Class EA) process categorizes proposed municipal projects according to their anticipated environmental impact, and requires increasingly stringent review requirements as the magnitude of the anticipated environmental impact increases.
- This project will be conducted in compliance with Schedule "C" of the Municipal Engineers Association "*Municipal Class Environmental Assessment,*" (October 2000, as amended 2007).

Municipal Class EA Process



Planning and Policy Context

<p>Switching Gears – Oakville Transportation Master Plan (2013)</p>		<ul style="list-style-type: none"> • Vision of creating a more balanced transportation system through transit, active transportation and road network improvements as well as travel demand management practices 	<p>Livable Oakville – Town of Oakville Official Plan (2009)</p>		<ul style="list-style-type: none"> • Established desired land use patterns within the Town of Oakville to 2031 in line with the Places to Grow Act 2005. • Discusses Bronte Village development in detail
<p>Oakville Active Transportation Master Plan (2009)</p>		<ul style="list-style-type: none"> • Goal to develop a comprehensive cycle and pedestrian network • Implement short term and long term actions to encourage AT as both a means of commuting and a recreational activity 	<p>Downtown Transportation and Streetscape Study (2015)</p>		<ul style="list-style-type: none"> • Looks at ways to improve downtown Oakville by maximizing facility space, enhancing accessibility, leveraging existing roadways, and increasing the cultural character
<p>Bronte Village Revitalization Study (2009)</p>		<ul style="list-style-type: none"> • Goal of study was to develop policies that respond to development pressure in Bronte Village and to help increase vibrancy of the local commercial area 	<p>Oakville Streetscape Strategy (2014)</p>		<ul style="list-style-type: none"> • Provides for streetscape studies to be undertaken within the Town of Oakville • Studies will be conducted by an inter-departmental project team with representation from all departments in the Town of Oakville
<p>Oakville Harbours Master Plan (ongoing)</p>		<ul style="list-style-type: none"> • Initiative incorporating Bronte Harbour and Oakville Harbour • Provides vision for harbours and their role in the community 	<p>Oakville Town-Wide Flood Study (2008)</p>		<ul style="list-style-type: none"> • Plan for the reduction of flood conditions at flood-prone sites

Environmental Inventories



Natural Environment

Aquatic Environment

- Three watercourses within the study area: *Bronte Creek, Fourteen Mile Creek and McCraney Creek*
- Correspondence with MNRF has indicated the following:
 - Bronte Creek is an occupied reach for American Eel and Silver Shiner
 - Fourteen Mile Creek is an occupied for Redside Dace
- DFO provided the following list of federally listed Species at Risk (SAR) within the study area:
 - ➔ American Eel, Silver Shiner (Under Consideration)
 - ➔ Deepwater Sculpin, Upper Great Lakes Kiyi (Special Concern)



Redside Dace



American Eel



Silver Shiner

Natural Environment

Natural Sciences

- A NHIC search indicated the presence of the following Environmentally Sensitive Areas in the study area:
 - ➔ Environmentally Sensitive Area identified within the valleylands of Bronte Creek – “Halton Region ESA No. 10 Bronte Creek”
 - ➔ Evaluated Provincially Significant Lower Bronte Creek Wetland Complex
- MNRF indicated that the following SAR have been recorded in the vicinity of the study area:
 - ➔ Red-Necked Grebe (S3B, S4N)
 - ➔ Snapping Turtle, Eastern Wood-Pewee, Northern Map Turtle (Special Concern)
 - ➔ Chimney Swift, Bank Swallow, Barn Swallow (Threatened, with general habitat protection)
- MNRF further indicated that the following Species At Risk have the potential to occur within the study area:
 - ➔ Butternut tree, Little Brown Bat, Northern Bat

Hydrology and Hydraulics / Stormwater Management

- Lakeshore Road crosses Bronte Creek, Fourteen Mile Creek and McCraney Creek
- Lakeshore Road receives drainage from existing development north of the right-of-way
- Lakeshore Road drainage system is urban (storm sewers), rural (ditches) and mixed (both sewers and ditches)
- No existing stormwater management within road corridor or within receiving drainage systems
- Drainage system assessment integrated with ongoing Town Storm Sewer Master Plan and Coronation Park Class EA



Stage 1 Archaeological Assessment (AA)

A Stage 1 AA was completed for the study corridor and identified the following:

- that 89% of the study area does not exhibit archaeological potential (previously disturbed or excessive slope)
- 11% of the study area has potential for archaeological resources and will require a Stage 2 Archaeological Assessment



Facing southwest along
Lakeshore Road West.
The manicured lawn retains
archaeological potential,
intensive testing required.

Built Heritage and Cultural Landscape Assessment

The Built Heritage and Cultural Landscape Assessment determined the following:

- This stretch of Lakeshore Road, although not designated as a Heritage Conservation District, contains numerous heritage resources including a church, a cemetery, a private school, residences and a marina building
- There are 12 listed heritage resources in the study area
- There are 7 designated heritage resources (properties) including St. Jude's cemetery and Acacia's House

St. Jude's –
rural 19th
century church
cemetery



Acacia's House
– built circa
1911

Transportation and Traffic Study

The traffic analysis was completed for existing (2016) and future (2021 and 2031) conditions. The key findings are:

- For the years 2016, 2021 and 2031 Lakeshore Road West operates at an acceptable level of service. No requirement for widening
- Corridor improvements are required for left turning vehicles to improve safety
- Intersection improvements are warranted at some locations to optimize efficiency
- Pedestrian and cycling infrastructure is required to provide connectivity throughout corridor, improve safety and promote active transportation
- The recommended configuration for Lakeshore Road West is a 3 lane cross-section, with one lane in each direction and a center two-way left turning lane



Road Safety Investigation

Goal

To examine the facility's safety performance, to identify areas or factors that are producing undue collision risk (if any), and to recommend infrastructure-based solutions to any identified problems

Key Findings

- 494 collisions in the study area from Jan 1st, 2006 to Dec 31st, 2016
- Slight decrease in collisions from year to year
- As expected, results show that more collisions occur through Bronte Village than throughout remainder of the study area (due to larger traffic volumes in the area)

Opportunities for Improvement

- Review the horizontal alignment of Lakeshore Road West
- Convert Lakeshore Road West to a three lane facility (one lane in each direction with center two-way left turning lane)
- Improve sidewalks on both sides of the road where possible
- Consider speed management measures such as roundabouts, narrow lanes, cycling lanes, roadside landscaping

Key Issues

- **Potential Corridor Improvements**
- **Bronte Village**
- **Drainage and Stormwater Management**
- **Active Transportation**

Potential Corridor Improvements

Triller Place Intersection

- ▶ Confirm if traffic signals are required at this location



Potential Corridor Improvements

Bronte Road Intersection

- ▶ Investigate options to address skew and improve road user safety



Potential Corridor Improvements

Sir John Colborne Recreation Centre for Seniors, east of Third Line

- ▶ Consider crossing options for improved road user safety
- ▶ Cyclist fatality in 2016



Potential Corridor Improvements

Appleby College

- ▶ Suffolk Avenue intersection operational constraints
- ▶ High number of Pedestrians



Bronte Village

Streetscaping and public realm improvements in Bronte Village area

► Key Issues

- streetscaping and public realm improvements in Bronte Village area
- need for access improvement at Bronte Harbour park
- parking opportunities in the commercial districts within the ROW
- provision of bike racks and other furnishing, wider pedestrian space



Bronte Village

Need for access improvement at Bronte Harbour park

- ▶ Key Issues
 - ▶ Harbours Master Plan objective is to provide recommendations on improved public access, better connections to surrounding neighbourhoods, and recreational amenities



Drainage and Stormwater Management

Address general drainage requirements and drainage area plans, including a SWM plan and backwater analysis for all structures

▶ **Key Issues**

- ▶ two major creek crossings within the study area (Bronte Creek and Fourteen Mile Creek)
- ▶ Rationalization of drainage systems
- ▶ Evaluate opportunities to incorporate Low Impact Development (LID) stormwater management elements into the road corridor improvements with urbanization of the roadway
- ▶ No existing stormwater management within road corridor or within receiving drainage systems
- ▶ Drainage system assessment integrated with ongoing Town Storm Sewer Master Plan and Coronation Park Class EA

Active Transportation

To provide a network of on and off road pedestrian and cycling facilities that allows for the use of active transportation modes as an alternative to the automobile (“Livable Oakville” (2009)).

▶ **Key Issues**

- ▶ Pedestrian safety at bridges within study area
- ▶ Crossing issues at various locations, e.g. Solingate Drive, Triller Place / West River Street and Coronation Park
- ▶ Discontinuous pedestrian facilities, e.g. east of Fourteen Mile Creek Bridge

Active Transportation

Pedestrian safety at bridges will be considered as follows:

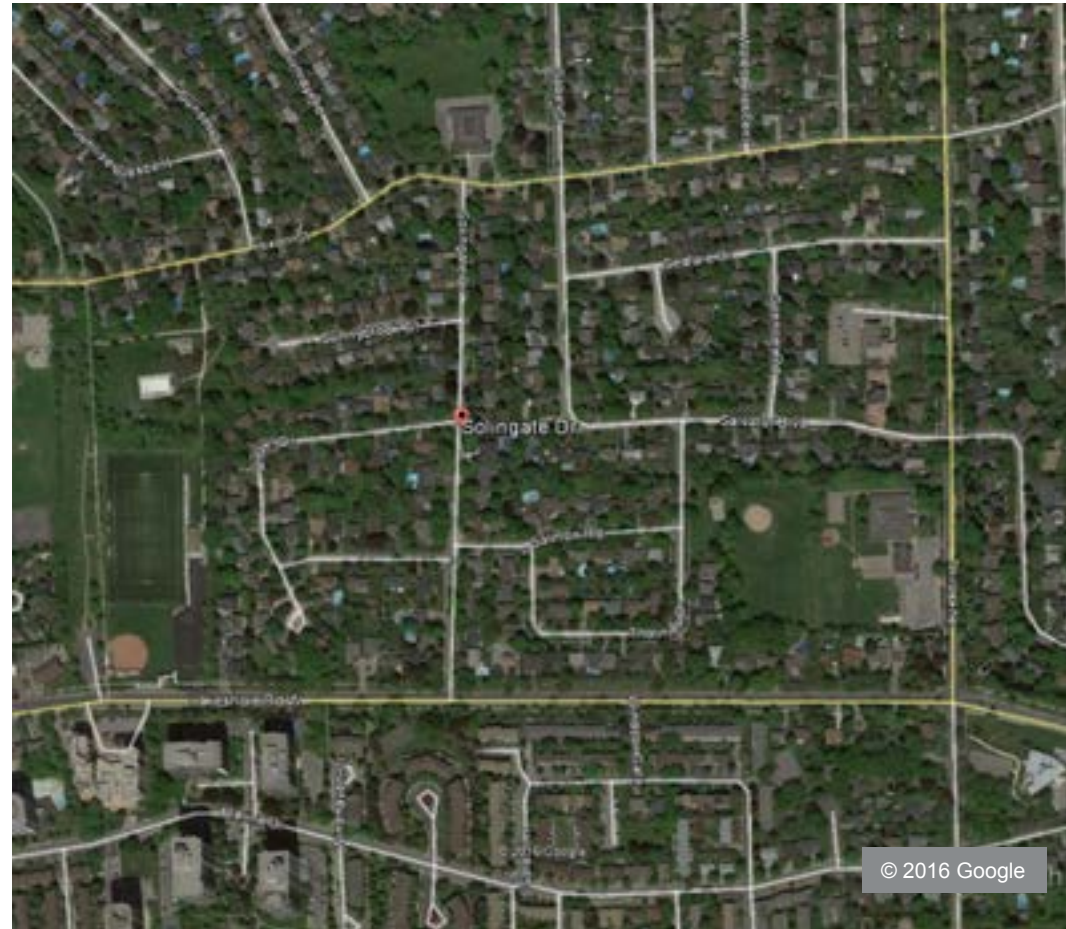
- ▶ sufficient width and separation from traffic
- ▶ continuity with adjacent facilities
- ▶ Accessibility and mobility needs (handrails, respite benches, etc.)



Active Transportation

Crossing issues at Solingate Drive

- ▶ Pedestrian Crossing options to improve user safety



Active Transportation

Triller Place / West River Street Intersection

- ▶ Pedestrian Crossing options to improve user safety



Active Transportation

Crossing issues at Coronation Park

- ▶ Pedestrian Crossing options to improve user safety



Active Transportation

East of Fourteen Mile Creek Bridge pedestrian sidewalk activity a safety concern

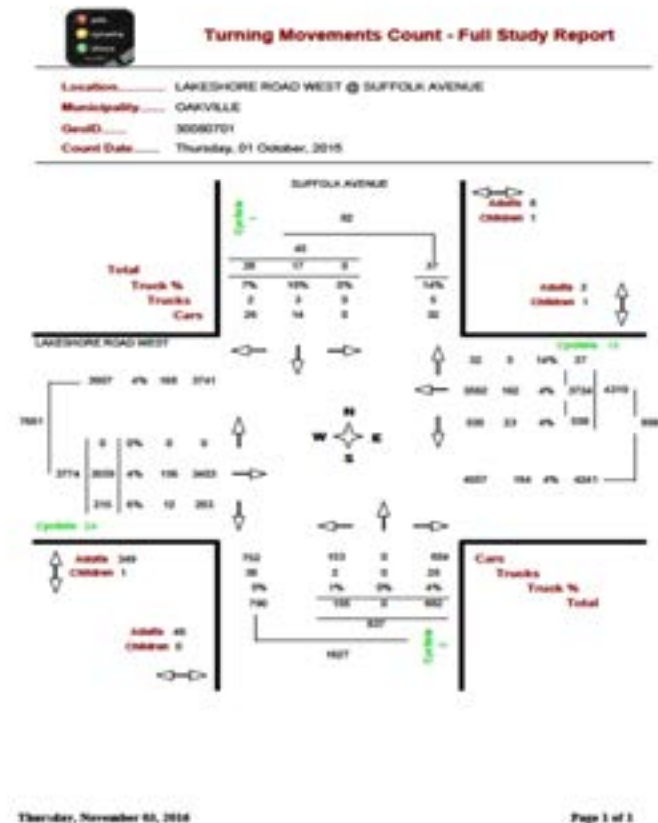
- ▶ clear zone between traffic and sidewalks is paved shoulder or bike lane
- ▶ Work was recently done at the structure



Potential Corridor Improvements

Appleby College

- ▶ Suffolk Avenue High number of pedestrians crossing Lakeshore Road West



Roundtable Discussion

▶ Next Steps

- ▶ Public information Centre No.1 - April 20, 2017
- ▶ Develop alternative design concepts
- ▶ Complete technical reports
- ▶ Stakeholder meeting No. 2 – fall 2017
- ▶ Public information Centre No.1 - fall 2017
- ▶ Completion – late winter 2018






Lakeshore Road West Class Environmental Assessment

PIC No. 1

Date: April 2017

								
			OPPORTUNITY	CONSTRAINT	OPPORTUNITY	CONSTRAINT	CONSTRAINT	
			Signal improvements, turn lanes	New development	Warrant for traffic signals	Pedestrian crossing	Designated heritage property Walton Memorial United Church. 2489 Lakeshore Road West	



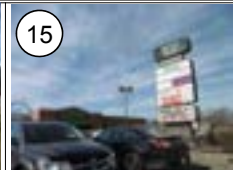










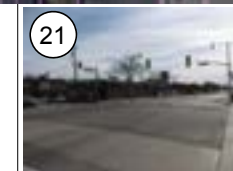
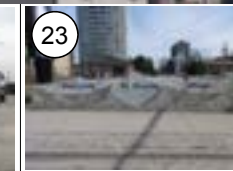


								
				CONSTRAINT	CONSTRAINT	CONSTRAINT	CONSTRAINT	OPPORTUNITY
				Listed heritage property Bronte Beach Park 3014 Lakeshore Road West	Bronte Creek Bridge	Bronte Harbour Yacht Club	Listed heritage property – Chris Vokes Memorial Park Bronte Cenotaph 2500 Lakeshore Road West	Bronte Harbour Marina

Lakeshore Road West Class Environmental Assessment

PIC No. 1

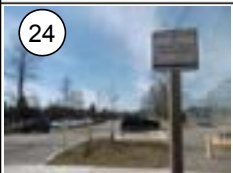




Date: April 2017

								
CONSTRAINT	CONSTRAINT	CONSTRAINT			OPPORTUNITY		CONSTRAINT	CONSTRAINT
Listed heritage property – Bronte Gore Park Bronte Cenotaph 2457 Lakeshore Road West	Gas station	Bronte Village Mall			Nelson Street signal improvements		Gateway feature	Controller box
								
								
OPPORTUNITY	OPPORTUNITY	OPPORTUNITY	OPPORTUNITY	OPPORTUNITY	CONSTRAINT/OPPORTUNITY		OPPORTUNITY	CONSTRAINT
Bronte Road intersection is skewed	Truck tracking from eastbound left turning trucks	Phased right turn	Jones Street signal improvements	Back of curb improvements, inconsistent 1m slash pad	On-street parking		East Street signal improvements	Bronte Village concrete sign








Lakeshore Road West Class Environmental Assessment




PIC No. 1

Date: April 2017




								
CONSTRAINT	CONSTRAINT	CONSTRAINT		CONSTRAINT				CONSTRAINT
Overflow parking for Oakville Community Residence	New school bus drop off area	Livno Common, new construction		New Apostolic Church				Listed heritage property. 2003 Lakeshore Road West



								
CONSTRAINT		CONSTRAINT	CONSTRAINT	CONSTRAINT	CONSTRAINT	CONSTRAINT		CONSTRAINT
Oakville Senior Citizen Residence		Trail to road, wooden fence	Mature trees and hydro poles	Bus pad	Fence	Windsor Court		Bike lane ends and goes north

								
OPPORTUNITY	CONSTRAINT					CONSTRAINT		
<ul style="list-style-type: none"> • Third Line signal improvements. • Sight line issues for turning vehicles 	Concrete sidewalk crossing					Pedestrian signal		







								
CONSTRAINT	CONSTRAINT	CONSTRAINT	OPPORTUNITY	CONSTRAINT	CONSTRAINT	CONSTRAINT		CONSTRAINT
Sir John Colbourne Park, new garden	Sir John Colbourne Seniors Center	Parkette garden	Improve storm water issues	New subdivision	New street – Hamlet Common	Coronation Park – Waterfront Trail		Listed heritage property- 1426 Lakeshore Road West



Lakeshore Road West Class Environmental Assessment

PIC No. 1

Date: April 2017

						
CONSTRAINT	CONSTRAINT		CONSTRAINT			CONSTRAINT
New waste water treatment plant	Access to waste water treatment plant		Kingdom Hall of Jehovah's Witnesses			New development






						
		OPPORTUNITY		OPPORTUNITY		
		Washout area – storm water improvements		Need for pedestrian crossing		

Lakeshore Road West Class Environmental Assessment

PIC No. 1

Date: April 2017

								
		OPPORTUNITY					CONSTRAINT	CONSTRAINT
		75 m no sidewalk, opportunity to improve pedestrian crossing					Wide shoulder	Westgate Park











								
CONSTRAINT		CONSTRAINT	OPPORTUNITY					
14 Mile Creek Bridge		Listed heritage property 1110 Lakeshore Road West	Improve storm water					

Lakeshore Road West Class Environmental Assessment

PIC No. 1

Date: April 2017

								
OPPORTUNITY	CONSTRAINT	CONSTRAINT	CONSTRAINT	CONSTRAINT		CONSTRAINT	CONSTRAINT	CONSTRAINT
Rebecca Street crossing	Designated heritage property- The Acacias, 573 Lakeshore Road West	Designated heritage property- Old McCraney House, 549 Lakeshore Road West	Okhee's corner store	Heritage listed property 491 Lakeshore Road West		West Wind Montessori School	Paliser Court - New Development	Designated heritage property- 105 Paliser Court






								
CONSTRAINT	CONSTRAINT	CONSTRAINT	OPPORTUNITY	OPPORTUNITY		CONSTRAINT		
McCraney Creek crossing	Raised island	Listed heritage property- Appleby College 540 Lakeshore Road West	Appleby College	Suffolk Avenue intersection improvements		Raised island		




Lakeshore Road West Class Environmental Assessment

PIC No. 1

Date: April 2017

							
<p>CONSTRAINT Listed heritage property- 369 Lakeshore Road West</p>			<p>CONSTRAINT St. Thomas Aquinas Catholic School</p>		<p>OPPORTUNITY Improvements to the Dorval Drive signalized intersection</p>		



							
<p>OPPORTUNITY Pedestrian issues, signalize</p>	<p>CONSTRAINT Listed heritage property – 372 Lakeshore Road West</p>				<p>CONSTRAINT Designated heritage property– St. Jude Cemetery 258 Lakeshore Road West</p>		



Lakeshore Road West Improvements Presentation to Councillors February 22, 2019



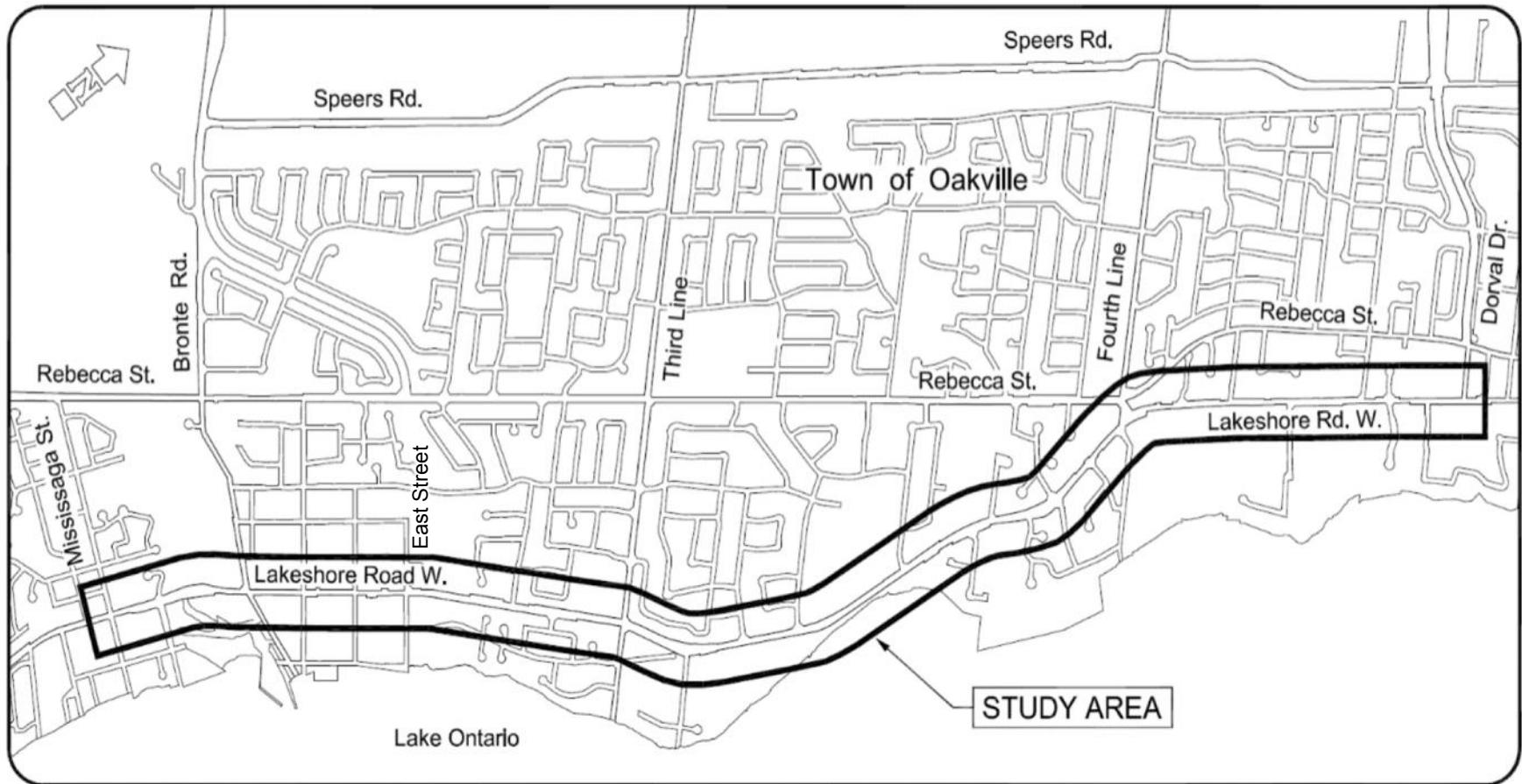
Agenda

1. Purpose of Meeting
2. EA Process & Council Direction
3. Principles Considered
4. Bicycle Facilities
5. Alternative Options
6. Feedback & Consultation Plan

EA PROCESS



Study Area



LAKESHORE RD. WEST
EXISTING

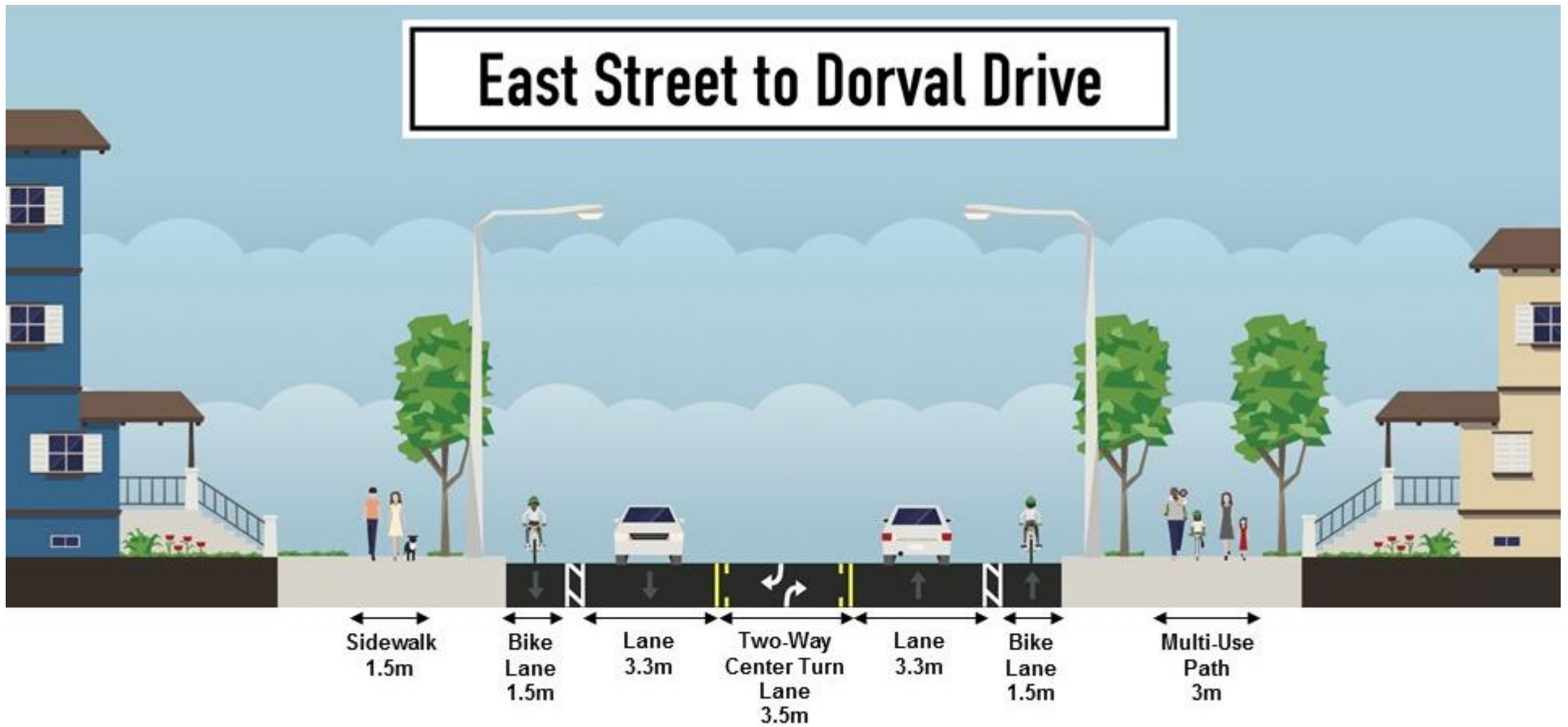
Legend



Council Direction

*“That staff be directed to complete additional consultation on the Lakeshore Road West Improvements (Mississaga Street to Dorval Drive) Class Environmental Assessment and report back to Council in early 2019, with recommendations that include at least one option reflecting **no continuous center turn lane, no loss of trees and no expropriation of property while maintaining cycle lanes and reflecting sidewalks/multiuse paths on at least one side.**”*

Previous Option



Key Principles Considered

Drainage deficiencies and storm water management

- Urbanize to curb & gutter cross-section with storm water system

Intersections improvements

- Additional turn lanes at several intersections recommended to improve traffic flow and safety

Continuous pedestrian and cycling facilities

- Study of protected bicycle lanes
- Multi-use trail for recreational users

Recognition of Scenic Corridor

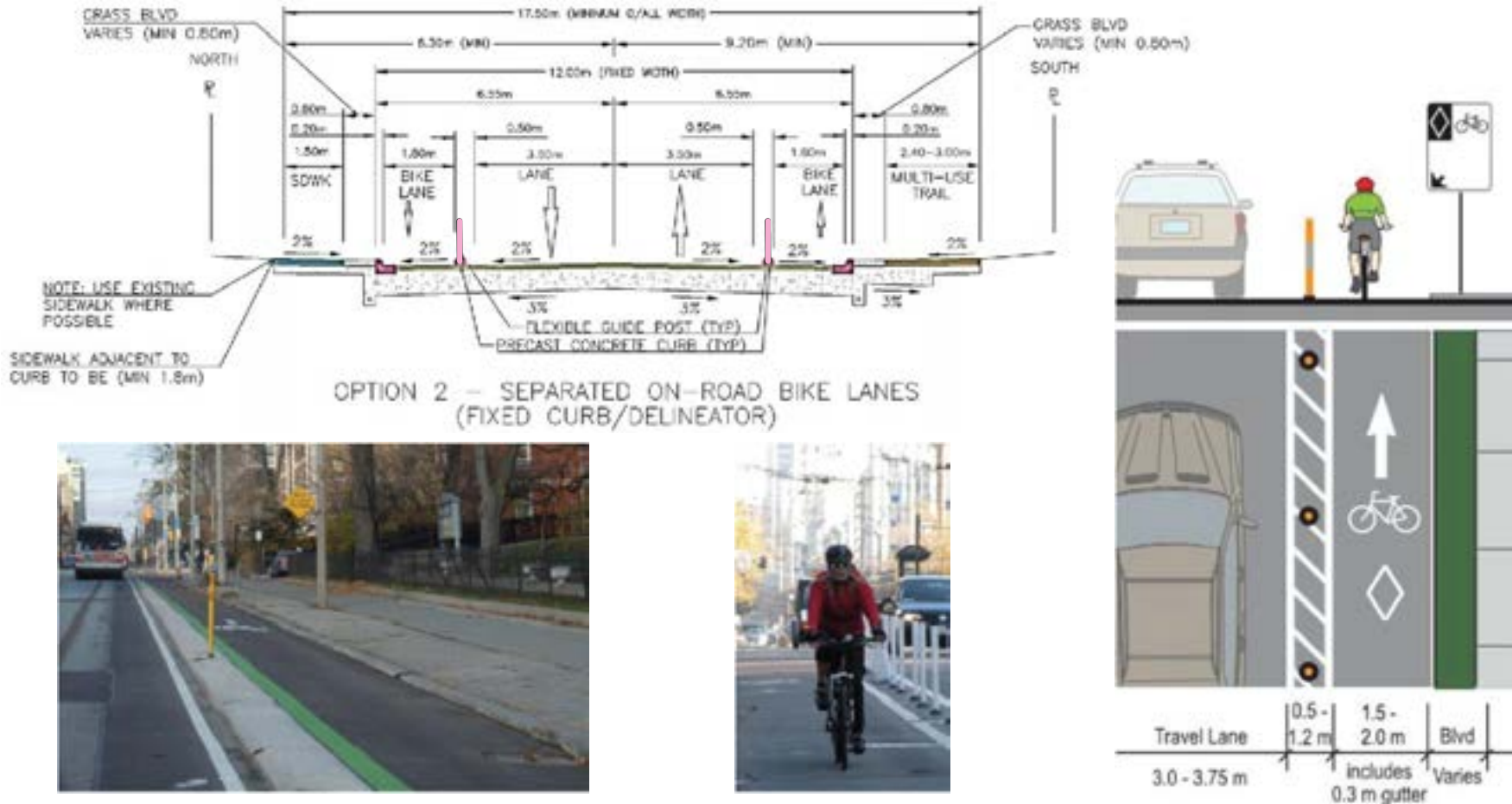
Protected Bike Lane Options

Evaluation Criteria

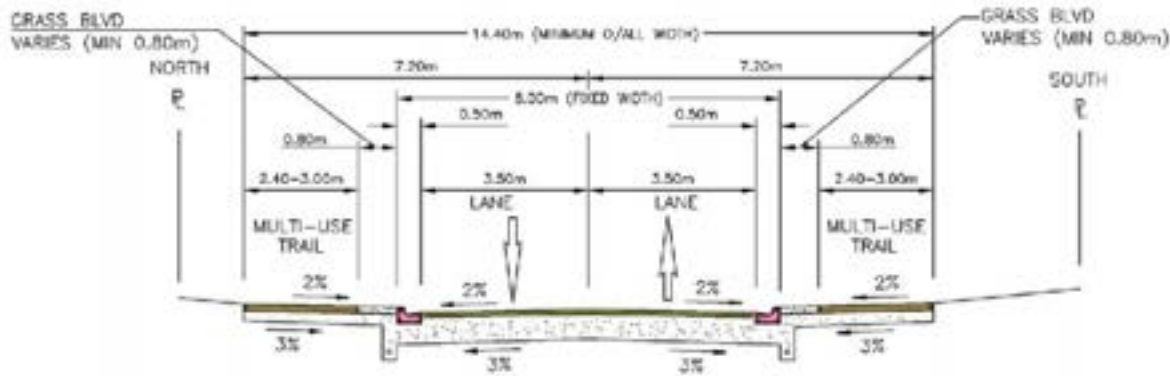
- Right-of-Way Requirements
- Road entrances conflicts
- Tree / Utility /Property Impacts
- Aesthetics / Scenic Corridor
- Bicycle/ Vehicular Traffic Operations Safety
- Capital and Maintenance Costs

4 Options Considered

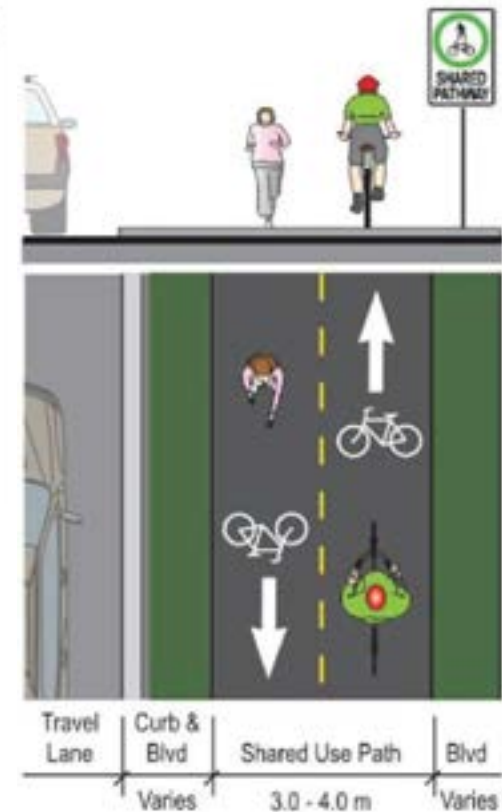
Option 2: Separated On-Road Bike Lanes (Fixed Curb/Delineator)



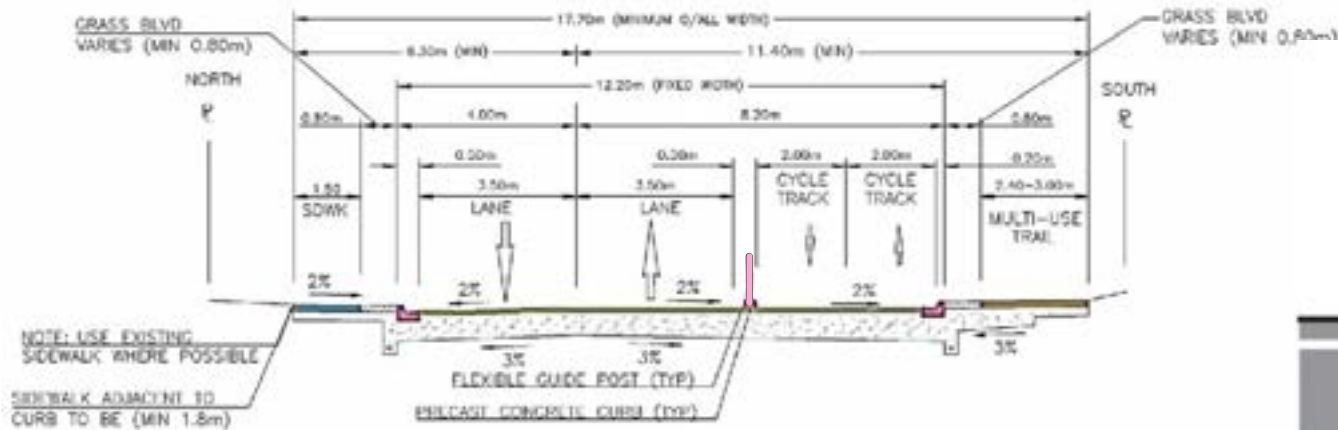
Option 3: Dual Multi Use Trails / No On-Road Bike Lanes



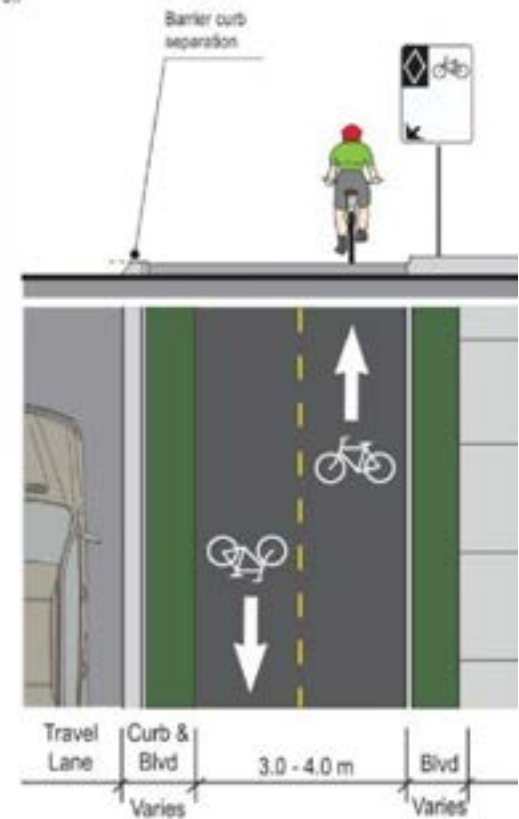
OPTION 3 – DUAL MULTI-USE TRAILS NORTH SIDE; SOUTH SIDE (4.00m)
NO ON-ROAD BIKE LANES



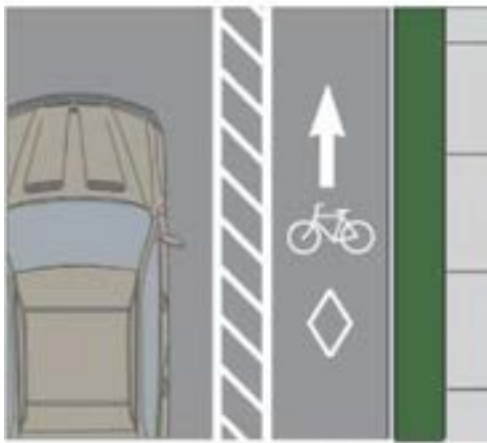
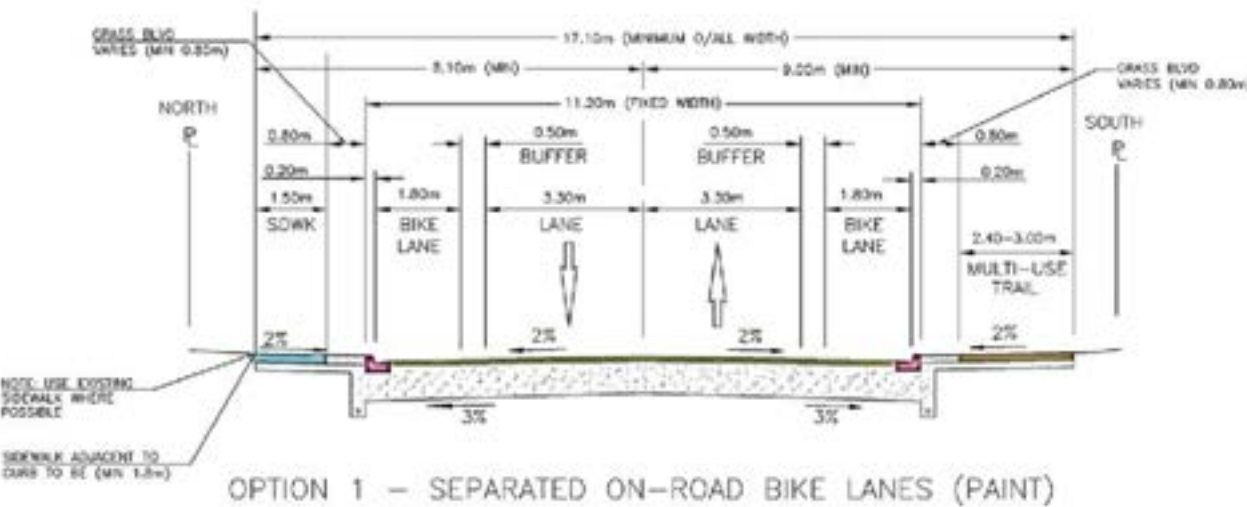
Option 4: Separated Two-Way Cycle-Track



OPTION 4 – SEPARATED TWO-WAY CYCLE TRACK (ONE SIDE)



Option 1 (PREFERRED): Separated On-Road Bike Lanes (Paint)



Travel Lane	0.5 - 1.2 m	1.5 - 1.8 m	Bivd
3.0 - 3.75 m	includes 0.3 m gutter		Varies



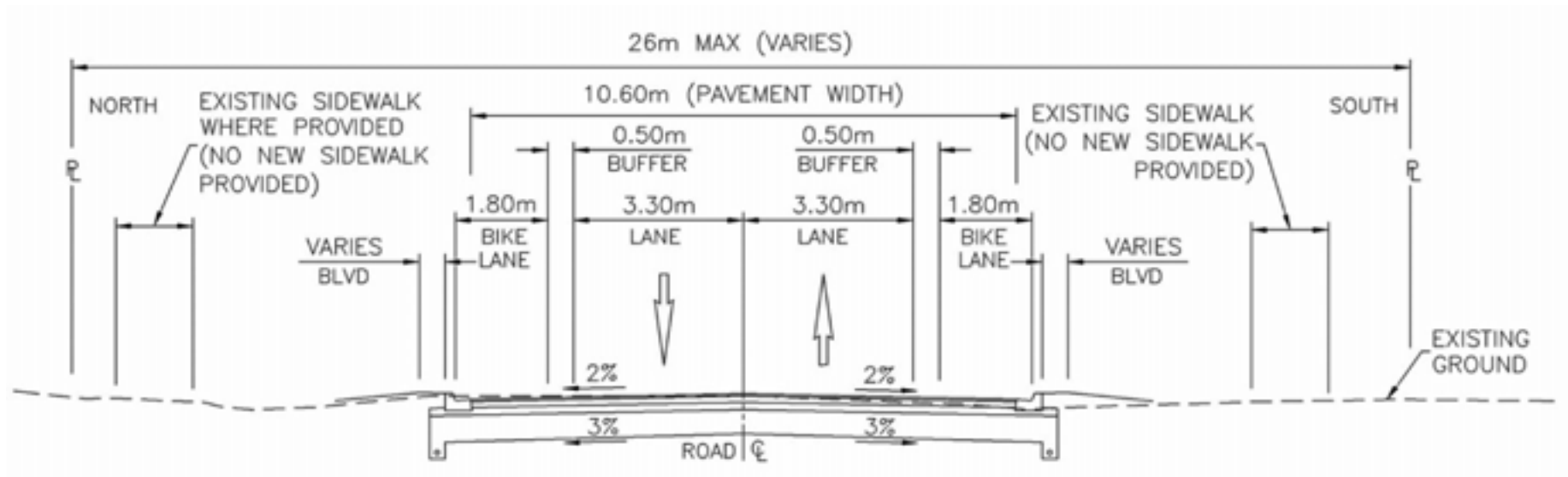
Segment Blocks

1. East to Third Line - .95 km
2. Third Line to Fourth Line – 2.5 km
3. Fourth Line to Dorval – 1.5 km



Alternative 1

Zero Impact Design



Alternative 1

Zero Impact Design

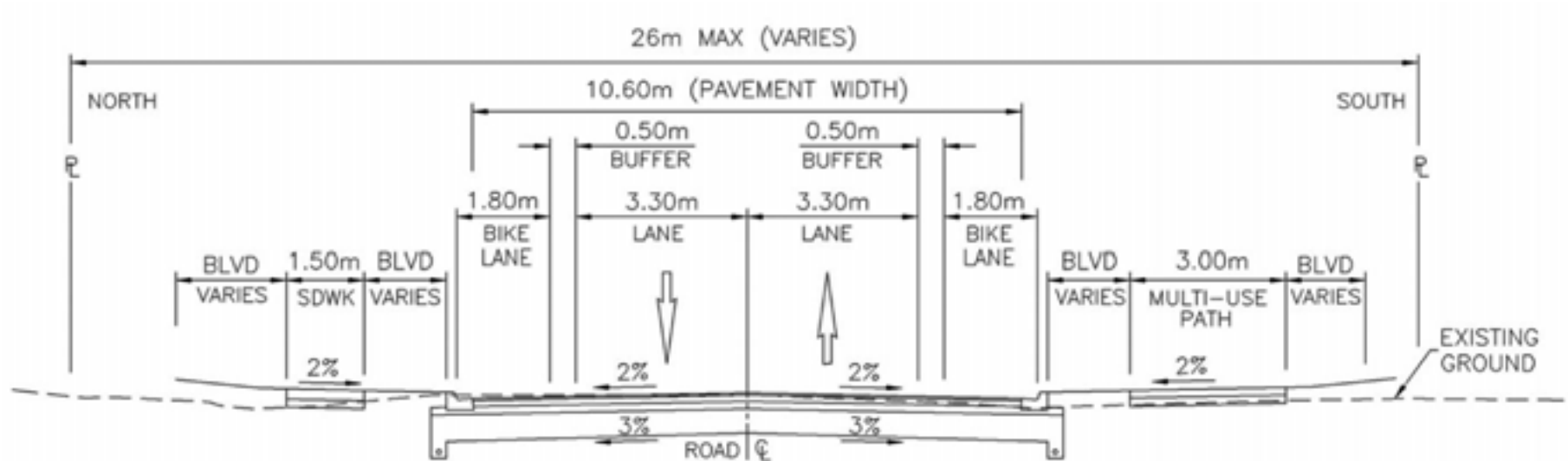
Evaluation Criteria	Factor	Block		
		East Street to Third Line	Third Line to Fourth Line	Fourth Line to Dorval Drive
Trees	# Removed >20 dbh	0	0	0
	# Removed </=20 dbh	0	0	0
Property Requirements	Total Land Required (m ²)	0*	0*	0*
Impermeable Surface	Increase of Impervious Surface (m ²)	132	-4,033	1,323
AT Infrastructure	Continuous AT Achieved (Y/N)	N	N	N
Intersections	Improvements Achieved (Y/N)	N	N	N





Alternative 2

Context Sensitive Design

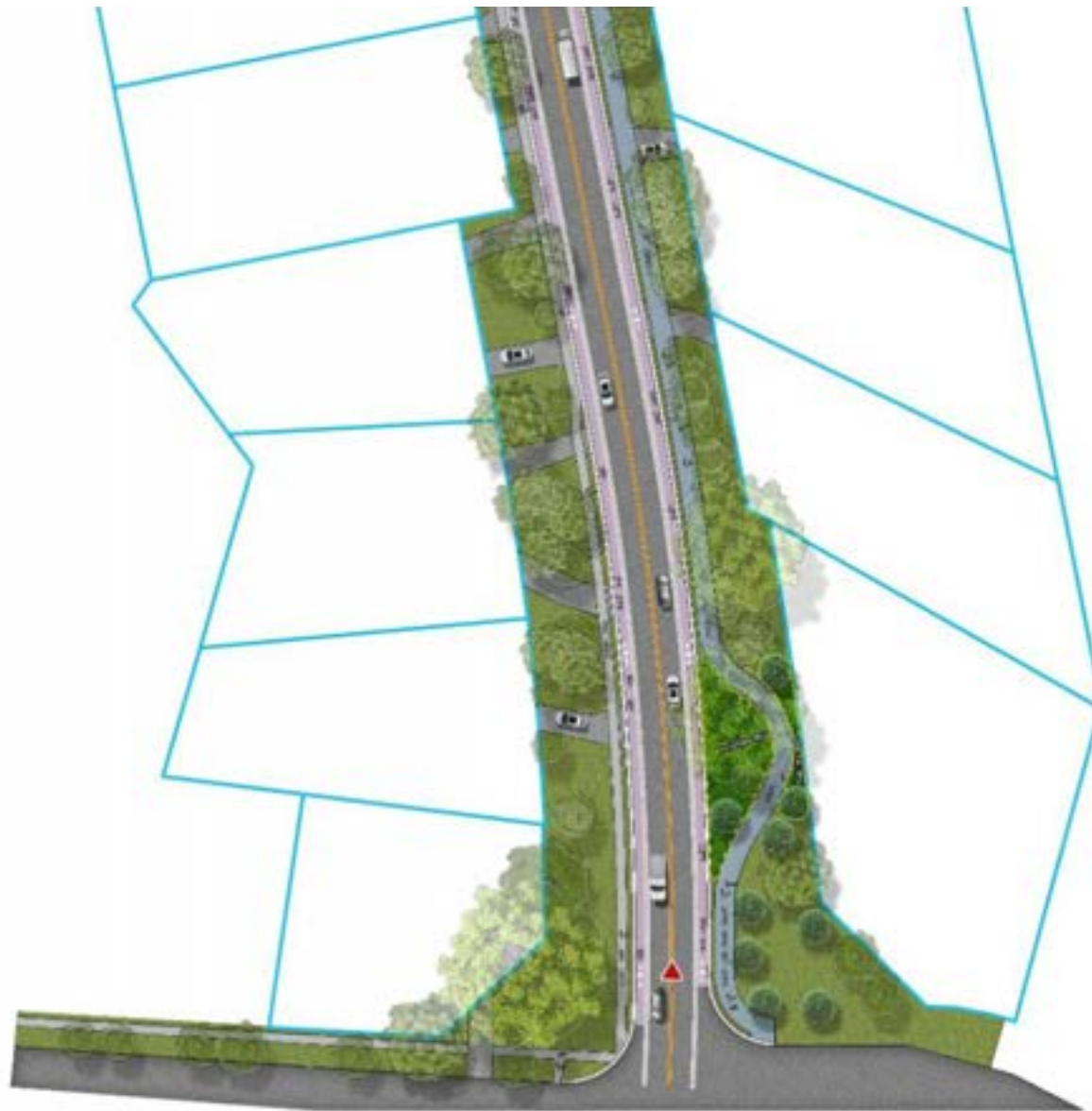


Alternative 2

Context Sensitive Design

Evaluation Criteria	Factor	Block		
		East Street to Third Line	Third Line to Fourth Line	Fourth Line to Dorval Drive
Trees	# Removed >20 dbh	30	20	45
	# Removed </=20 dbh	7	22	77
Property Requirements	Total Land Required (m ²)	0*	0*	123*
Impermeable Surface	Increase of Impervious Surface (m ²)	2,324	773	5,689
AT Infrastructure	Continuous AT Achieved (Y/N)	Y	Y	Y
Intersections	Improvements Achieved (Y/N)	Y	Y	Y













SPRING GARDEN ROAD

WILDER DRIVE

TREE LEGEND

-  EXISTING TREES
-  PROPOSED TREES

'PROPOSED SURFACES'

-  VEHICULAR ROADWAY
-  CONC. SWK
-  BLVD. LAWN'S
-  BIKE LANE
-  MULTI USE PATH
-  PERSPECTIVE VIEW POINT

Alternative 2A

Mix by Block



Alternative 2:
Context Sensitive
Design

Alternative 1:
Zero Impact, but
with Intersection
Improvements

Alternative 2A

Mix by Block

Evaluation Criteria	Factor	Block		
		East Street to Third Line	Third Line to Fourth Line	Fourth Line to Dorval Drive
Trees	# Removed >20 dbh	30	20	2
	# Removed </=20 dbh	7	22	0
Property Requirements	Total Land Required (m ²)	0*	0*	123*
Impermeable Surface	Increase of Impervious Surface (m ²)	2,324	773	530
AT Infrastructure	Continuous AT Achieved (Y/N)	Y	Y	N
Intersections	Improvements Achieved (Y/N)	Y	Y	Y

Alternative Summary

Evaluation Criteria	Factor	Alternative 1: Zero Impact Design	Alternative 2: Context Sensitive	Alternative 2A: Mix by Block
Trees	# Removed >20 dbh	0	95	52
	# Removed </=20 dbh	0	106	29
Property Requirements	Total Land Required (m ²)	0*	123*	123*
Impermeable Surface	Increase of Impervious Surface (m ²)	-2,578	8,786	3,627
AT Infrastructure	Continuous AT Achieved (Y/N)	N	Y	Partial
Intersections	Improvements Achieved (Y/N)	N	Y	Y

Discussion

- **Feedback on Alternative Options**
- **Consultation Plan**

Mcandrew, Louise

From: Felker, Bob
Sent: April-01-19 5:34 PM
To: Syed Rizvi
Cc: 'Mark Covert (mark.covert@oakville.ca)'; Sinke, David; Smith, Neal; Mcandrew, Louise
Subject: RE: Lakeshore Road West EA - Stakeholders Meeting Presentation
Attachments: 01 Lakeshore Rd EA Stakeholder Meet Miss_East_Third 20190329.pdf; 02 Lakeshore Rd EA Stakeholder Meet Third_Fourth 20190401.pdf; 03 Lakeshore Rd EA Stakeholder Meet Fourth_Dorval 20190401.pdf

Syed, as requested I have attached the attendance sheets from the 3 sessions.

Bob

Bob Felker

O: 519-650-7139

M: 226-751-3854

From: Syed Rizvi [mailto:syed.rizvi@oakville.ca]
Sent: April-01-19 5:10 PM
To: Cathy Duddeck <cathy.duddeck@oakville.ca>; Ray Chisholm <ray.chisholm@oakville.ca>; Beth Robertson <beth.robertson@oakville.ca>; Sean O'Meara <sean.o'meara@oakville.ca>
Cc: Jill Stephen <jill.stephen@oakville.ca>; Mark Covert <mark.covert@oakville.ca>; Sinke, David <david.sinke@woodplc.com>; Felker, Bob <bob.felker@woodplc.com>; Smith, Neal <neal.smith@woodplc.com>; Jill MacInnes <jill.macinnes@oakville.ca>
Subject: Lakeshore Road West EA - Stakeholders Meeting Presentation

Hello everyone,

Thank you to all who attended one of our stakeholder meetings on March 29 and April 1. We appreciate your input in the roadway design process.

Attached is the presentation that was shared at the meetings, which outlines design alternatives for the Lakeshore Road West improvements. Presentation will be posted on the [town's website](#) before end of the day tomorrow.

We are planning to hold a public information meeting in May to share the recommended option with residents and gather further input. Notice of the meeting will be mailed to area residents and posted on our website in the coming weeks. Concept plans of the recommended option will be made available for review prior to the public information meeting.

If you have any further comments or questions prior to the public meeting, please don't hesitate to contact me.

Thanks
Syed

Syed Rizvi, M.Sc., P. Eng
Transportation Engineer

Engineering & Construction

Town of Oakville | 905-845-6601, ext.3981 | www.oakville.ca

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<http://www.oakville.ca/privacy.html>

Lakeshore Road West Improvements Stakeholder Meetings

Mississauga Street to Third Line - March 29, 2019
Third Line to Fourth Line - April 1, 2019 AM
Fourth Line to Dorval Drive - April 1, 2019 PM



Agenda

1. Purpose of Study
2. EA Process
3. Council Direction
4. Protected Bike Lane Options
5. Key Principles of Alternative Designs
6. Corridor Blocks and Road Improvement Alternatives
7. Discussion
8. Next Steps

Purpose of Study

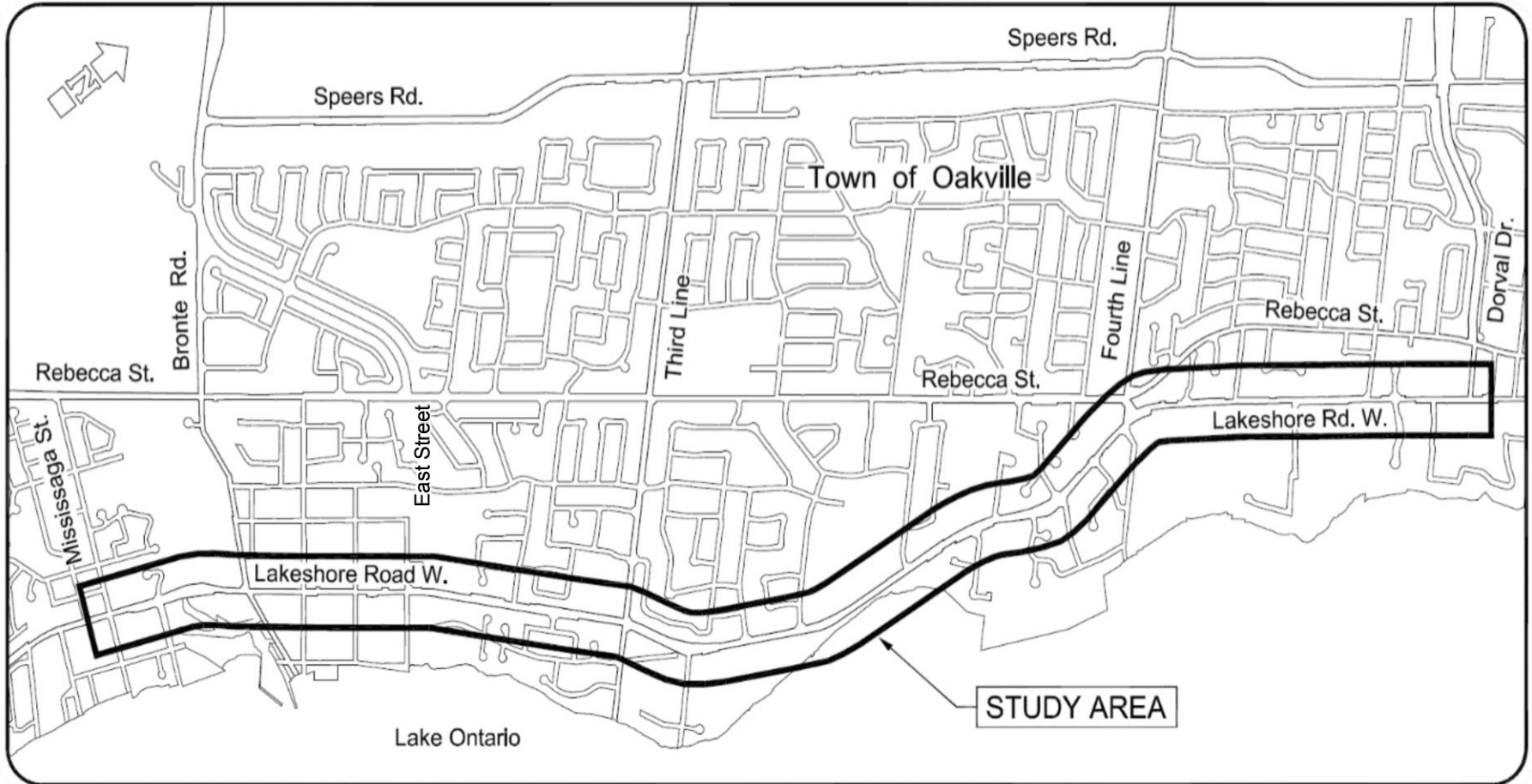
Improve

- Pedestrian and cyclist safety and accessibility
- Traffic safety
- Pavement conditions
- Drainage and storm water management
- Intersection safety and operations
- Roadway design to meet current standards
- Creek crossings and structures

EA Process



Study Area



Rendering 1 Existing Conditions



Rendering 2 Existing Conditions



Council Direction

Resolution from August 7, 2018 Council Meeting

*“That staff be directed to complete additional consultation on the Lakeshore Road West Improvements (Mississaga Street to Dorval Drive) Class Environmental Assessment and report back to Council in early 2019, with recommendations that include at least one option reflecting **no continuous centre turn lane, no loss of trees and no expropriation of property while maintaining cycle lanes and reflecting sidewalks/multi use paths on at least one side and minimizing impervious surfaces.**”*

Protected Bike Lane Options

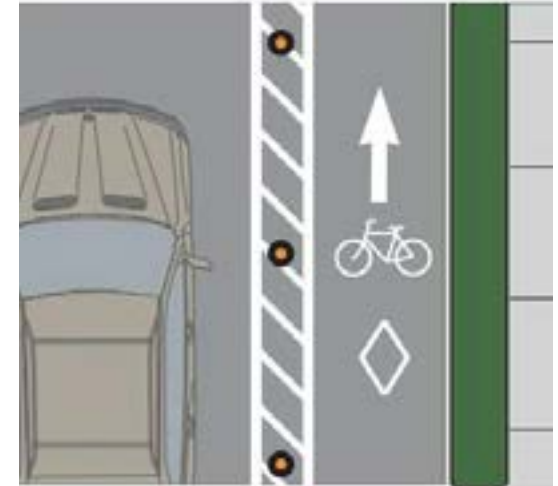
Evaluation Criteria

- Right-of-Way / Tree / Utility / Property Impacts
- Aesthetics / Scenic Corridor
- Bicycle / Vehicle Interactions
- Capital and Maintenance Costs

Designs Considered

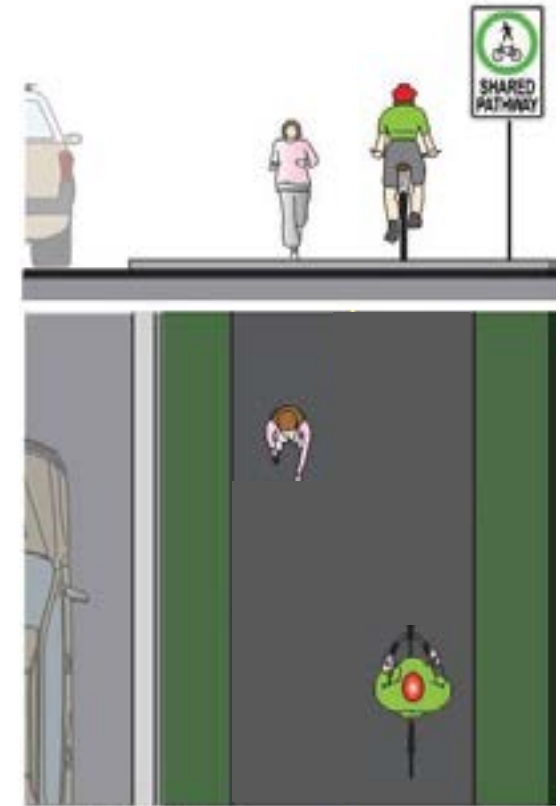
- On-Road Bike Lanes with Fixed Curb/Delineator
- Dual Multi-Use Paths (No On-Road Bike Lanes)
- Two-Way Cycle-Track
- On-Road Bike Lanes (Painted Buffer)

On-Road Bike Lanes with Fixed Curb/Delineator



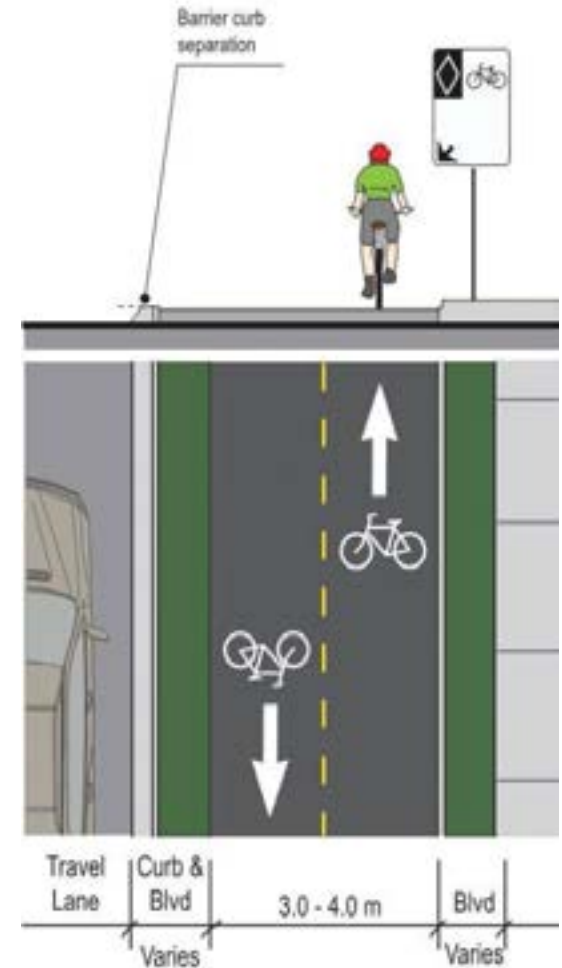
Travel Lane	0.5 - 1.2 m	1.5 - 2.0 m	Blvd
3.0 - 3.75 m		includes 0.3 m gutter	Varies

Dual Multi-Use Paths (No On-Road Bike Lanes)

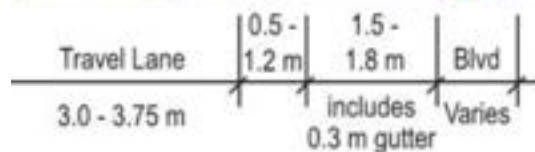
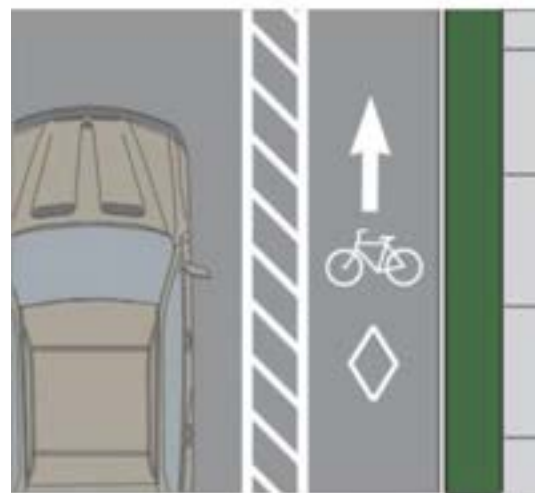


Travel Lane	Curb & Blvd	Shared Use Path	Blvd
	Varies	3.0 - 4.0 m	Varies

Two-Way Cycle-Track



On-Road Bike Lanes with Painted Buffer (Preferred)



Key Principles of Design Reassessment

Take public's concerns into consideration

- Minimize tree & property impacts

Continuous pedestrian and cycling facilities

- On-Road Bike Lanes with Painted Buffer
- Multi-use path for recreational users

Intersections improvements

- Additional turn lanes at intersections to improve traffic flow and safety

Drainage deficiencies and storm water management

- Urbanize to curb & gutter cross-section with storm water system
- Address flooding issues

Recognition of Scenic Corridor

Alternative Designs and Impacts

Key principles

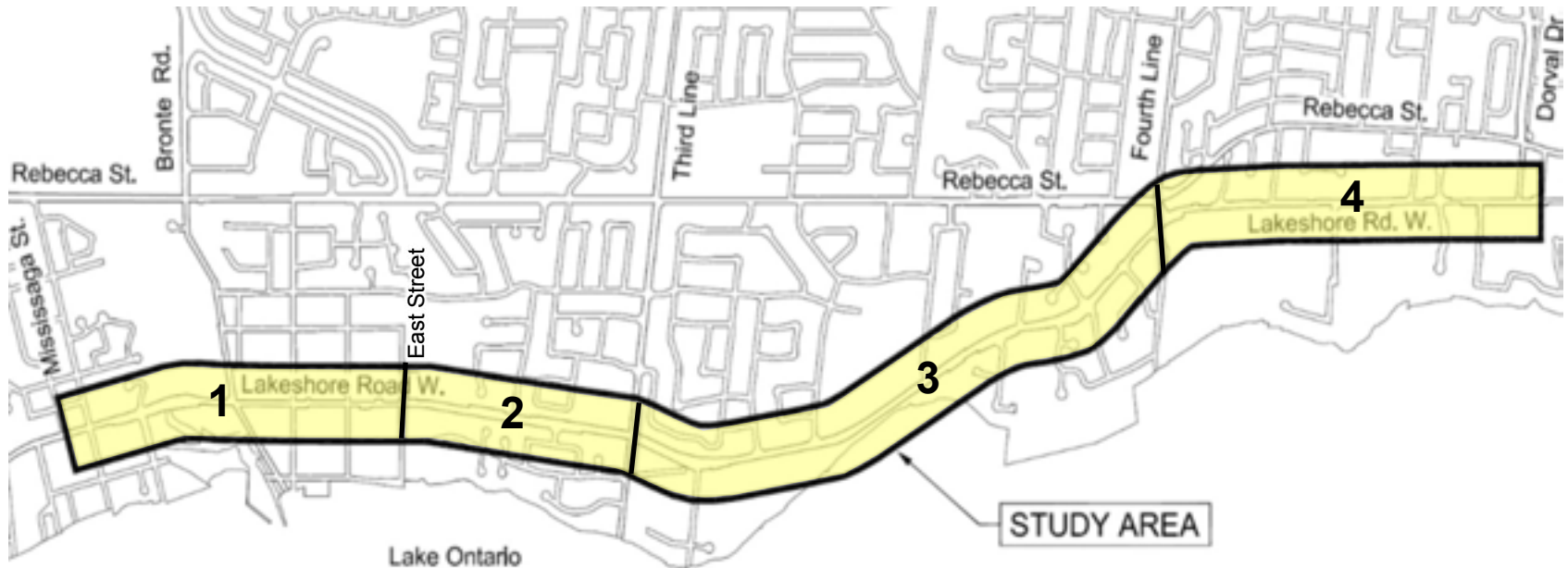
- Continuous bike lanes
- Continuous/accessible sidewalks
- Multi-use trails for all users
- Intersection improvements

To achieve these important goals, some tree removals and property impacts will result

Alternative designs developed strive to find a balance

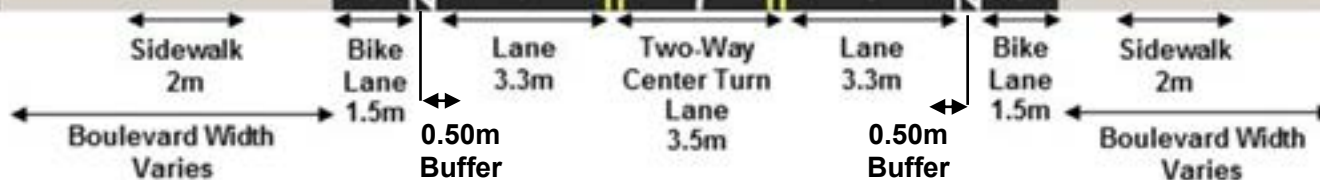
Corridor Blocks

1. Mississaga Street to East Street – 1.3 km
2. East Street to Third Line – 1.0 km
3. Third Line to Fourth Line – 2.5 km
4. Fourth Line to Dorval Drive – 1.5 km



Block 1 – Previous Preferred Design (no change)

Mississauga Street to East Street



Block 1 – Mississauga Street to East Street

Factor	Impact
Potential Tree Removals >20 dbh	11*
Potential Trees Removals </=20 dbh	9*
Total Land Required (m ²)	2,657
Increase of Impervious Surface (m ²)	0
Multi-use Path (South Side)	N/A
On-road Bike Lanes	Y
Continuous Sidewalk	Y
Drainage Improvements	N/A
Intersection Improvements	Y

*Does not include trees within the Bronte BIA area which may be preserved or replaced, subject to streetscaping design

DESIGN ALTERNATIVES - BLOCKS 2, 3 & 4

Centre turn-lane removed

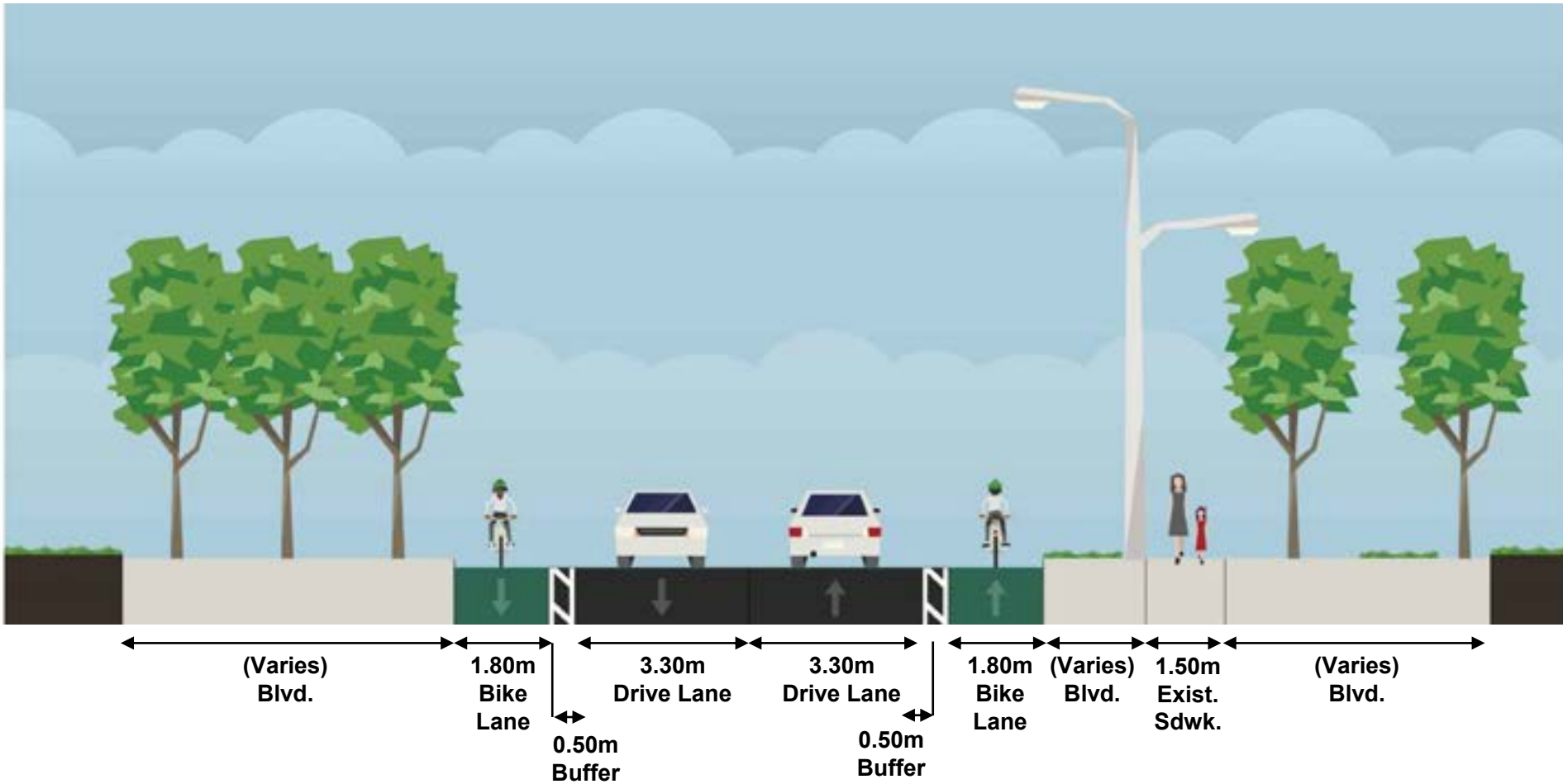
Alternative Design A – No Impact

- Urbanize to curb & gutter cross-section with storm water system
- No centre-turn lane
- On-road bike lanes, sidewalks where they exist

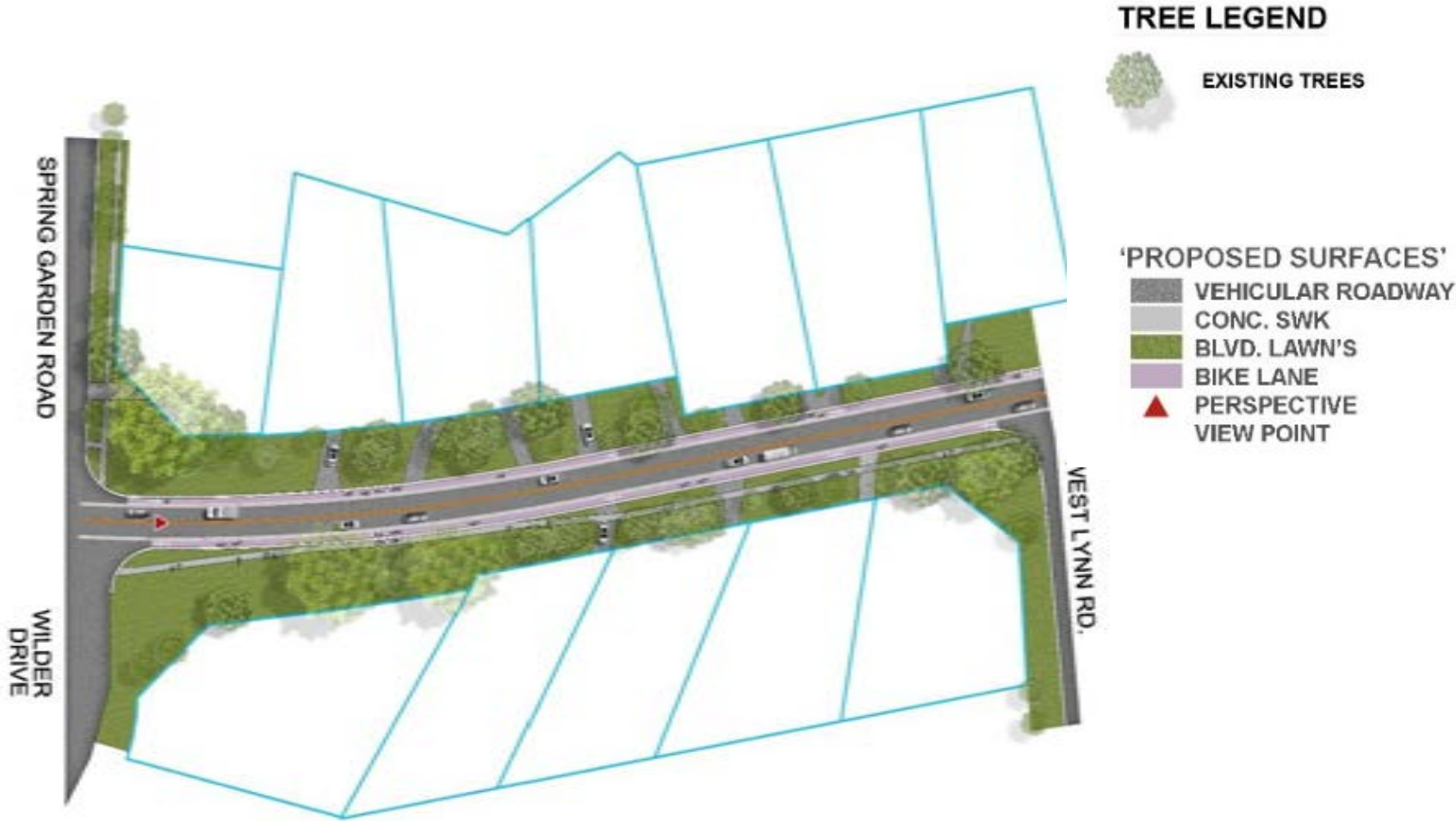
Alternative Design B – Hybrid

- Urbanize to curb & gutter cross-section with storm water system
- No centre-turn lane
- On-road bike lanes, continuous sidewalks, multi-use path
- Improve intersections

Alternative Design A – No Impact



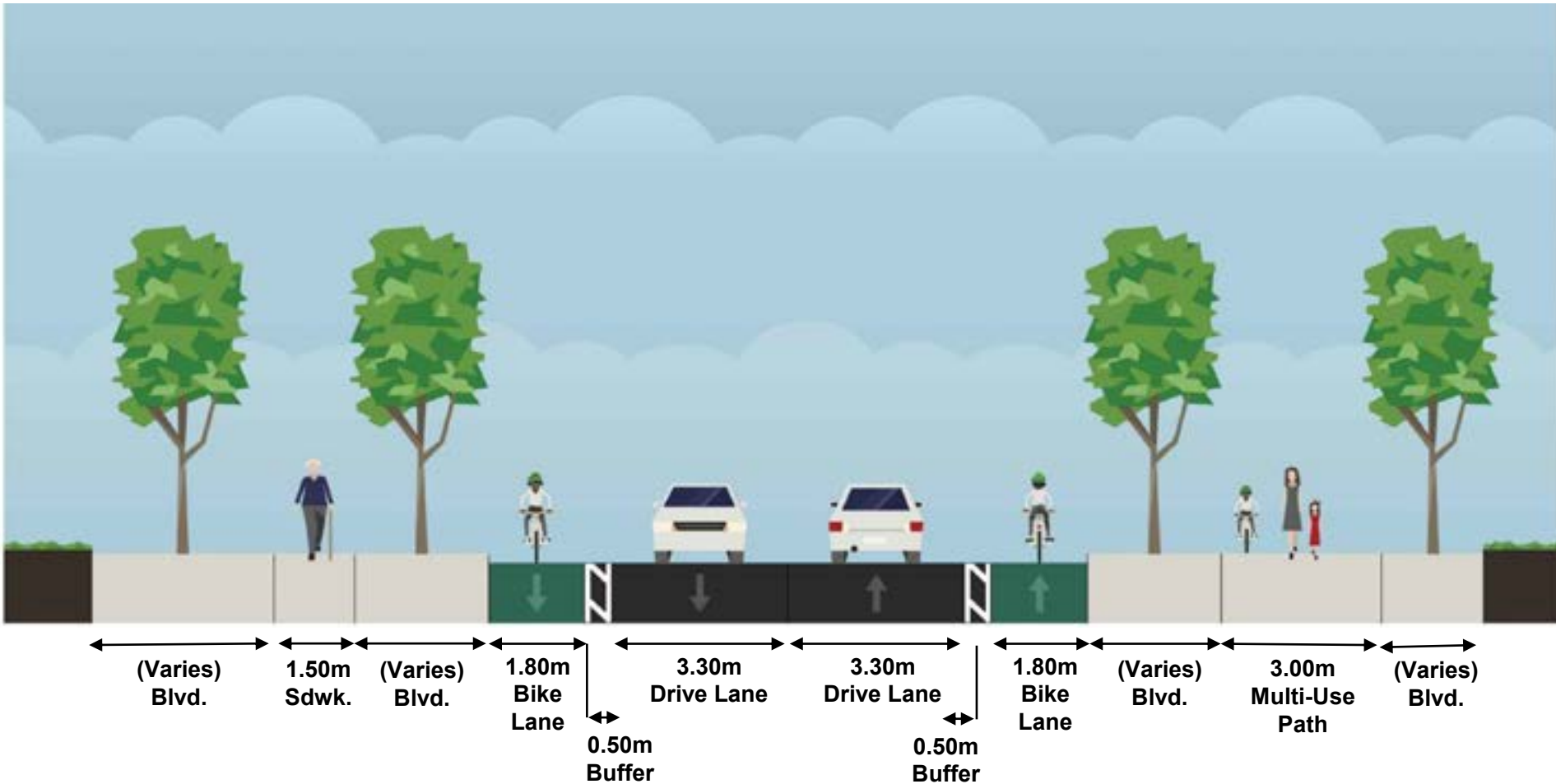
Alternative Design A – No Impact



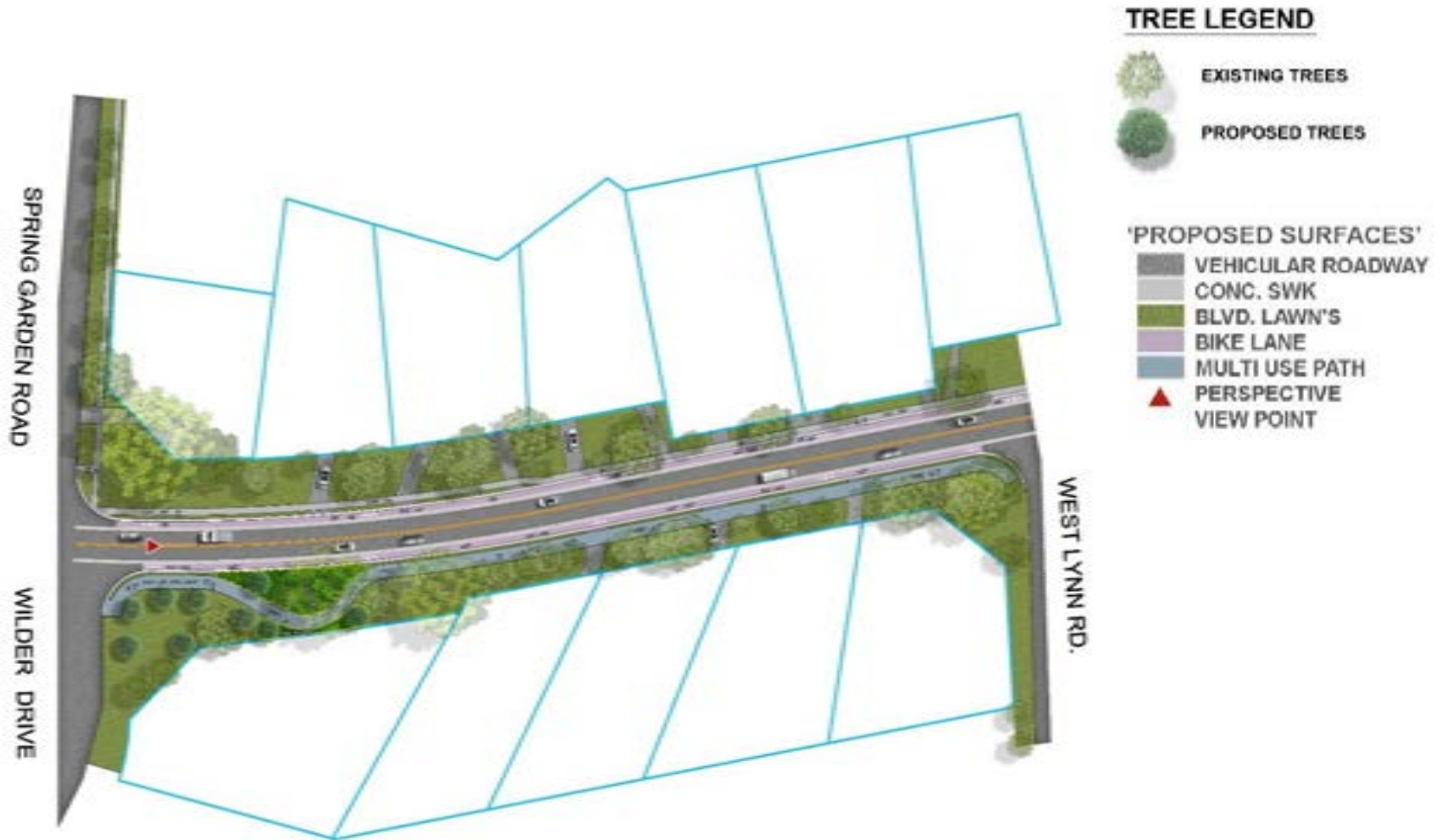
Alternative Design A – No Impact



Alternative Design B - Hybrid



Alternative Design B - Hybrid



Alternative Design B - Hybrid



Block 2 Evaluation – East Street to Third Line

Factor	Alt Design A No Impact	Alt Design A1* Minimal Impact	Alt Design B Hybrid
Potential Tree Removals >20 dbh	0	10**	30
Potential Trees Removals </=20 dbh	0	5**	7
Total Land Required (m ²)	0	0	0
Increase of Impervious Surface (m ²)	132	1,000	2,324
Multi-use Path (South Side)	N	N	Y
On-road Bike Lanes	Y	Y	Y
Continuous Sidewalk (North Side)	N	Y	Y
Drainage Improvements	Y	Y	Y
Intersection Improvements	N	Y	Y

*Alternative Design A1 adds completion of sidewalks gaps and intersection improvements

** Estimated – to be confirmed

Alternative Design B - Hybrid



Block 3 Evaluation – Third Line to Fourth Line

Factor	Alt Design A No Impact	Alt Design A1* Minimal Impact	Alt Design B Hybrid
Potential Tree Removals >20 dbh	0	5**	20
Potential Trees Removals </=20 dbh	0	5**	22
Total Land Required (m ²)	0	0	0
Increase of Impervious Surface (m ²)	-4,033	-3,211	773
Multi-use Path (South Side)	N	N	Y
On-road Bike Lanes	Y	Y	Y
Continuous Sidewalk (North Side)	N	Y	Y
Drainage Improvements	Y	Y	Y
Intersection Improvements	N	Y	Y

*Alternative Design A1 adds completion of sidewalks gaps and intersection improvements

** Estimated – to be confirmed

Alternative Design B - Hybrid



Block 4 Evaluation – Fourth Line to Dorval

Factor	Alt Design A No Impact	Alt Design A1* Minimal Impact	Alt Design B Hybrid
Potential Tree Removals >20 dbh	0	9**	45
Potential Trees Removals </=20 dbh	0	8**	77
Total Land Required (m ²)	0	0	123
Increase of Impervious Surface (m ²)	1,323	2,906	5,689
Multi-use Path (South Side)	N	N	Y
On-road Bike Lanes	Y	Y	Y
Continuous Sidewalk (North Side)	N	Y	Y
Drainage Improvements	Y	Y	Y
Intersection Improvements	N	Y	Y

*Alternative Design A1 adds completion of sidewalks gaps and intersection improvements

** Estimated – to be confirmed

Alternative Design A1 – Minimal Impact



Alternative Designs and Impacts

Recommended Alternatives achieves

- ✓ Continuous bike lanes
 - ✓ Continuous/accessible sidewalks
 - ✓ Intersection improvements
 - ✓ Addresses drainage issues
-
- Multi-use trail from Mississauga to Fourth Line (not completed through to Dorval)

Discussion

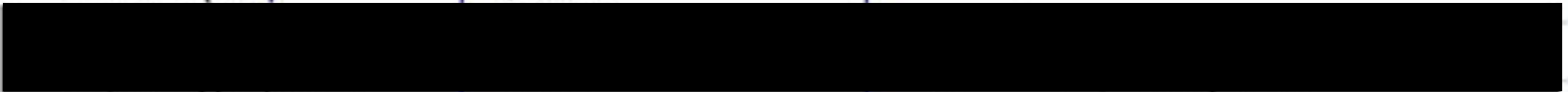
- **Feedback on Alternative Designs**
- **Next Steps**
- **Thank you!**

MARCH 28, 2019

LAKESHORE RD EA STEERING COMMITTEE MEET # 2

MISSISSAGA TO EAST ST & TO THIRD LINE

NAME	ORG	EMAIL OR PHONE
Rev James Gill WARREN CALDWELL	Walton United Church WALTON UNITED CHURCH	jgwaltonuc@yahoo.ca jw.caldwell@outlook.com
CATHY DUDDECK RAY CHISHOLM	COUNCIL COUNCIL	
Sean O'Meara	'	
PAUL CATES	BRONTE BIA	paucatesbbg@yahoo.com
BETH ROBERTSON	COUNCIL	
NEAL SMITH	WOOD (CONSULTANTS)	neal.smith@woodplc.com
BOB FELKER	WOOD	bob.felker@woodplc.com
Jeff Kennedy	Oakville Christian	jkennedy@oakvillechristianschool.com
Jill Stephen	Town	



JILL MACINNES	Town	jillmacinnes@oakville.ca
Christina Tizzard	Town	
MARK CONYER	TOWN	

LAKESHORE RD. EA STAKEHOLDER MEETING #2

APRIL 1, 2019

THIRD LINE TO FOURTH LINE

NAME	ORG	EMAIL OR PHONE
BOB FELKER	WOOD EGIS	bob.felker@woodplc.com
NEAL SMITH	WOOD EGIS	neal.smith@woodplc.com
SYED RIZVI	Town of Oakville	Syed.rizvi@oakville.ca
JILL MACINNES	" "	jill.macinnnes@oakville.ca
JILL STEPHENSON	" "	jill.stephen@oakville.ca

DON COX

CORCORAN PARK R.H.

DONALD-C-COX@SYDNEY.CA

LAKESHORE RD EA STAKEHOLDER MEETING #2 APRIL 1, 2019
FOURTH LINE TO DORVAL DRIVE

NAME	ORG	PHONE OR EMAIL
BOB FELKER Negl Smith	WOOD EIS WOOD EIS	bob.felker@woodplc.com Negl.Smith@woodplc.com
[REDACTED]		
Syed Rizvi Dwight Kennedy	Town of Oakville Appleton College	Syed.Rizvi@oakville.ca dkenned@appleton.ca
[REDACTED]		
CLARK SWALE Innes van Nestrand JILL MACINNIS	MARKET LANE " " TOWN	905 208 2355 (905) 845-4681 ivannestrand@ appleton.ca jill.macinnis@appleton.ca
[REDACTED]		

Minutes

Date: Friday, March 29, 2019, 10 am start **Meeting at:** Oakville Town Hall, Trafalgar Room

Ref: TPB166147.1802 Lakeshore Road Improvements EA

Subject/purpose:

Stakeholder Meeting #2 – Corridor Section Mississaga Street to East Street/ East Street to Third Line

Attendees:

Councillor O'Meara, Ward 1
Councillor Robertson, Ward 1
Councillor Chisholm, Ward 2
Councillor Duddeck, Ward 2
Jill Stephen, Town of Oakville
Mark Covert, Town of Oakville
Syed Rizvi, Town of Oakville
Jill MacInnes, Town of Oakville
Christina Tizzard, Town of Oakville
Neal Smith, Wood
Bob Felker, Wood

Rev. James Gill, Walton United Church
Warren Caldwell, Walton United Church
Paul Cates, Bronte BIA
Jeff Kennedy, Oakville Christian School
[REDACTED], Bronte Resident

To be presented/discussed:

ACTION BY:

Opening Remarks

Mark Covert (MC) with the Town of Oakville (Town) welcomed everyone to the session to discuss progress with the Lakeshore Road West Improvements Class EA Study.

Introductions were made. It was noted that this is the first of 3 meetings with Stakeholders. the presentation material is the same, but the invited attendees and the focus is on three Corridor Blocks with this first meeting looking at the road section from Mississaga Street to Third Line.

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To be presented/discussed:

ACTION BY:

1. Purpose of Study

MC reviewed the meeting Agenda and provided an overview of the purpose of the EA study to improve, pedestrian and cyclist accessibility and safety, traffic safety (particularly intersection improvements), pavement conditions, drainage and stormwater management, creek crossings and structures, and to bring the roadway up to current design standards.

2. EA Process

The EA Process was outlined and it was stressed that in reflecting what was heard at the Public Meeting held in July last year that we take a step back and re-assess the proposed design. It is recognized that safety and capacity are major concerns. The focus of the re-assessment is on the section of the Lakeshore Road West corridor from East Street to Dorval Drive. For this session we are also discussing the section of the Lakeshore Road West corridor from Mississaga Street to East Street that includes Bronte Village.

The re-assessment identified and evaluated alternative designs, and a preferred design is being proposed that has been altered from that previously presented.

3. Existing Conditions

Images showing existing road conditions were presented. Features and conditions of note included; the scenic nature of the corridor, the poor pavement conditions, gaps in sidewalks and on-road bike lanes, and the open drainage swales.

4. Council Direction

The resolution passed by Town Council at its August 7, 2018 meeting was reviewed. The intent was to report back to Council in early 2019, but the timeframe now is to report to Council in early September, 2019. This will allow for the additional consultation Council has requested.

Also as directed, an option has been developed with no continuous centre turn lane, no loss of trees and no expropriation of property. The option does include continuous on road bike lanes while maintaining and minimizing



To be presented/discussed:

ACTION BY:

impervious surfaces. It maintains the existing sidewalk on the south side of the corridor.

5. Protected Bike Lane Options

The Town indicated that Council also asked that options for protected bike lanes be assessed and four types of facility were evaluated:

-) On-Road Bike Lanes with Fixed Curb/Delineator
-) Dual Multi-Use Paths (No On-Road Bike Lanes)
-) Two-Way Cycle-Track
-) On-Road Bike Lanes with Painted Buffer

MC Reviewed the evaluation criteria and the pros and cons for the 4 options. The preferred option - On-Road Bike Lanes with Painted Buffer was carried forward for all the roadway alternatives being re-assessed.

6. Key Principles of Design Reassessment

MC described the key principles that were applied in carrying out the re-assessment of the design alternatives. They include recognition of the scenic corridor, having continuous sidewalk/multi-use path and on road bike lanes, intersection improvements for traffic flow and safety, and urbanize to curb and gutter to improve stormwater management.

7. Alternative Designs and Impacts

MC outlined the approach taken for the development of the alternative designs being presented for discussion. The intent is to try to achieve a balance by including intersection improvements, continuous on road bike lanes, continuous and accessible sidewalks, and a multi-use path for all users, while respecting the scenic nature of the corridor, and minimizing impacts to trees and property.

8. Corridor Blocks

MC Outlined the corridor blocks that make up the Lakeshore Road West corridor under study:

-) Mississaga Street to East Street – 1.3 km
-) East Street to Third Line – 1.0 km



To be presented/discussed:

ACTION BY:

-) Third Line to Fourth Line – 2.5 km
-) Fourth Line to Dorval Drive – 1.5 km

He noted that is important when looking at the impacts associated with the design alternatives to keep in mind the overall length of the road segments being assessed.

9. Block 1 – Mississaga Street to East Street (incl. Bronte Village)

For the block from Mississaga Street to East Street the preferred design as presented last year has retained the same cross section with three lanes of traffic, including the centre left turn lane, on road bike lanes with painted Buffer, and sidewalks on both sides.

Syed Rizvi (SR) indicated that consideration has been given to the Bronte Village land use plans and development proposals, including the redevelopment of the Bronte Village Mall. Design improvements for this block include the re-design of the Bronte Road intersection with Lakeshore Road West to reduce the skew and shorten the crosswalk lengths. Although it was noted that the positioning of buildings and driveway access, limits space and flexibility to achieve an ideal perpendicular alignment.

There was considerable discussion about existing conditions at the Bronte Road intersection, and the need for traffic flow and safety improvements, and measures to safely accommodate pedestrians. Rev. Gill (JG) stressed that Walton Church is a community hub that has active programming 7 days a week. He noted that the concerns with traffic flow and access are shared by the residents of the Bronte Harbour Club Condominium (2511 Lakeshore Rd. W.) and the users of the Bronte Harbour Yacht Club.

Warren Caldwell (WC) commented that the Bronte Intersection is simply unsafe and that is supportive of the rebuild and redesign.

Councillor O'Meara (SO'M) noted that the Town has worked on optimized signal programming for congregation leaving times. He also noted that, as others had mentioned, the current lane configuration going from 3 lanes to four lanes, and back again leads to unsafe lane switching and creates a bottleneck.



To be presented/discussed:

ACTION BY:

Clarification was given that the roadway on the bridge will be reduced from 4 lanes to 2 lanes, while retaining the on road bike lanes with painted buffer and the existing sidewalks. The left turn lane will be re-instated easterly from the bridge to Bronte road to provide turn in access to the Condo, Church and Yacht Club. SR noted that the soon to be constructed Wyecroft Road bridge is planned to provide some traffic relieve on Lakeshore Road West, and that change-over of the lane configuration on this section of Lakeshore is contingent of completion of the Wyecroft Road bridge.

Councillor Robertson (BR) noted that the current roadway at Bronte Intersection is dangerous for cyclists, and that the new design will be much safer.

10. Block 1 – Mississaga Street to East Street Evaluation Summary

MC described the impacts associated with the preferred design for the Mississaga Street to East Street corridor block. There will be some tree removals, mostly in the road section from Mississaga Street to Bronte Road. There will be some tree removals in the Bronte Village section to East Street, but tree removals will be replaced with trees in pits (Silva Cell), as part of a broader streetscaping plan.

MC also noted that some property will need to be acquired, primarily between Bronte Road and East Street, with small portions of frontages needed, mostly at intersections to accommodate the design improvements.

11. Design Alternatives - Blocks 2, 3 & 4

MC then introduced the re-assessment of design alternatives carried out for the 3 sections of roadway corridor from East Street to Dorval Drive. The Traffic Study was updated to further assess the need for a continuous left turn lane, given the degree of impact to trees and property. It was determined that while there may be some safety improvements, overall the improvements to traffic flow were minor. Accordingly, for this portion of the corridor, the 3 lane design alternative with the centre left turn lane was not carried forward.



To be presented/discussed:

ACTION BY:

Alternative Design A – No Impact

MC described Alternative Design A, the No Impact design, which consists of two 3.30m traffic lanes, continuous on road bike lanes with the painted buffer, and the existing sidewalk on the south side. It includes the conversion to curb and gutter. This alternative does not include a multi-use path, and the gaps in sidewalk on the north side remain.

Alternative Design A1 – Minimal Impact

This Alternative was developed from the No Impact design to fill in the gaps on the north side to provide a continuous sidewalk from East Street to Dorval Drive.

Alternative Design B – Hybrid

Alternative Design B carries forward the two 3.30m traffic lanes, and continuous on road bike lanes with the painted buffer, and includes the conversion to curb and gutter. With the alternative the existing sidewalk on the south side is converted to a multi-use path, and the gaps are filled on the north side to provide a continuous sidewalk from East Street to Dorval Drive.

The question was asked if the Town was trying to minimize including sidewalks, and Christine Tizzard (CT) responded that this is not the case, and that policies are in place to require sidewalks to increase mobility, enhance accessibility for all users, and improve health for residents.

12. Block 2 Evaluation - East Street to Third Line

MC presented the impact evaluation table for the 3 alternatives for the Block from East Street to Third Line. It was noted that the tree removals in this and other sections come mostly as the result adding sidewalk and the multi-use path. At the detail design stage various mitigation measures can be applied to reduce impacts, and retain more trees.

SO'M noted that it is important to have sidewalks for students to safely walk to and from schools, and that Alternative A still has the sidewalk gaps between Solingate Drive and Third Line.

Councillor Chisholm (RC) stressed that it will be helpful to show where trees can be planted to offset tree removals, and provide for overall streetscape



To be presented/discussed:

ACTION BY:

enhancement. Jill Stephen indicated that this is normally part of detail design, but the intent can certainly be presented, and documented as part of the mitigation measures to be implemented.

WC asked about the intersection improvements for this Block, and SR stated that the improvements would be the addition of right turn lanes on Third Line. WC also asked how the Multi-Use Path (MUP) would be connected at intersections. SR said that the MUP would transition to a wider sidewalk, with a marked crosswalk at the intersection, and then transitioning back to a paved MUP.

MC noted that the Staff Recommendation for the Block 2 section from East Street to Third Line is for Alternative B, the Hybrid Design.

13. Block 3 Evaluation – Third Line to Fourth Line

MC presented the impact evaluation table for the 3 alternatives for the Block from Third Line to Fourth Line. It was noted that there is a reduction in the impervious surface for both the No Impact and Minimal Impact Alternatives due to the fact that there are paved shoulders that would be removed with the transition to a 2 lane roadway with curb and gutter.

The Staff recommendation for the Block 3 section from Third Line to Fourth Line is for Alternative B, the Hybrid Design.

14. Block 4 Evaluation – Fourth Line to Dorval

MC presented the impact evaluation table for the 3 alternatives for the Block from Fourth Line to Dorval Drive. It was noted that the tree removals documented for Alternative B – the Hybrid Option are resulting from the installation of the Multi-use Path on the south side of the roadway, as well as the intersection improvements.

The Staff recommendation for the Block 3 section from Fourth Line to Dorval is for Alternative A1, the Minimal Impact options that includes continuous sidewalks on both sides of the roadway, and intersection improvements at Morden Avenue, and Dorval Drive.

██████████ asked if a traffic signal was considered for the Suffolk Avenue intersection at Appleby College. It was noted that the issue is with



To be presented/discussed:

ACTION BY:

morning eastbound traffic making the left turn into the college. JS responded that a full traffic signal was not warranted, however, the design calls for a pedestrian signal crosswalk.

15. General Comments

MC concluded the formal presentation and opened the floor for general comments.

Jeff Kennedy (JK) said that he was hoping to talk to about the design of intersections, in particular the Third Line intersection. His concerns are with students being able to safely walk and bike to school. SR responded that the focus of this meeting was to seek comment on the corridor options to be presented to the public, and less on specific design treatments. However, SR did note that the roundabout option was eliminated at Third Line and that improvements are proposed for cyclist and pedestrian safe use, and the addition of right turn lanes on Third Line.

It was noted by JG that the Town has made some positive changes in the Bronte Village area with the installation of the pedestrian crosswalk by the Oakville Senior Citizens Residence (OSCR), and the large stop sign at the Jones intersection.

WC asked about the impact of the Bronte Mall redevelopment and the addition of potentially 1000 residents would have on traffic and the design. CD indicated that a traffic study has to be done by the developer to identify the site specific impacts.

MC spoke about next steps, saying that there are two more meetings with stakeholders to discuss the corridor segments. Smaller “Kitchen Table” meetings are an option to take a look at site specific issues. A meeting with the public is proposed to happen sometime in May. This meeting, and the two remaining, are useful for Staff to know what the key concerns are, and what information is of importance to present to the public.

JS said that following the Public Meeting, Staff will work with the Consultant to finalize the Environmental Study Report (ESR) that documents the planning and design process, and the public input received. A recommendation report will then be taken to Council, likely in September, with a request to file a



To be presented/discussed:

ACTION BY:

Notice of Completion of the EA Study, and make the ESR available for the mandatory public review period.

16. Adjourn

The meeting adjourned with thanks to the Attendees.

Meeting Minutes prepared by:

Wood Environment & Infrastructure Solutions
a Division of Wood Canada Limited

Per: Bob Felker

BF/

Attachments:

Presentation – Lakeshore Road West Improvements Stakeholder Meetings



Minutes

Date: Monday, April 1, 2019, 10 am start **Meeting at:** Oakville Town Hall, Palermo Room

Ref: TPB166147.1802 Lakeshore Road Improvements EA

Subject/purpose:

Stakeholder Meeting #2 – Corridor Section Third Line to Fourth Line

Attendees:

Councillor Chisholm, Ward 2	[REDACTED], Lakeshore Road Resident
Councillor Duddeck, Ward 2	Don Cox, Coronation Park Residence Association
Jill Stephen, Town of Oakville	Pamela Knight, Coronation Park Residence Association
Mark Covert, Town of Oakville	[REDACTED], Lakeshore Road Resident
Syed Rizvi, Town of Oakville	
Jill MacInnes, Town of Oakville	
Neal Smith, Wood	
Bob Felker, Wood	

To be presented/discussed:

ACTION BY:

1. Opening Remarks

Mark Covert (MC) with the Town of Oakville (Town) welcomed everyone to the session to discuss progress with the Lakeshore Road West Improvements Class EA Study.

Introductions were made. It was noted that this is the second of 3 meetings with Stakeholders. the presentation material is the same, but the invited attendees and the focus is on three Corridor Blocks with this second meeting looking at the road section from Third Line to Fourth Line

2. Purpose of Study

MC reviewed the meeting Agenda and provided an overview of the purpose of the EA study to improve, pedestrian and cyclist accessibility and safety, traffic safety (particularly intersection improvements), pavement conditions, drainage and stormwater management, creek crossings and structures, and to bring the roadway up to current design standards. He noted that the re-

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ACTION BY:

assessment is being done at a high level to compare alternatives against key evaluation criteria such as tree protection and limiting property requirements.

Councillor Chisholm (RC) noted as an example that the re-assessment does not deal with the design specifics at intersections in detail.

3. EA Process

The EA Process was outlined and it was stressed that in reflecting what was heard at the Public Meeting held in July last year that we take a step back and re-assess the proposed design. It is recognized that safety and capacity are major concerns. The focus of the re-assessment is on the section of the Lakeshore Road West corridor from East Street to Dorval Drive.

The re-assessment identified and evaluated alternative designs, and a preferred design is being proposed that has been altered from that previously presented.

4. Existing Conditions

Images showing existing road conditions were presented. Features and conditions of note included; the scenic nature of the corridor, the poor pavement conditions, gaps in sidewalks and on-road bike lanes, and the open drainage swales.

5. Council Direction

The resolution passed by Town Council at its August 7, 2018 meeting was reviewed. The intent was to report back to Council in early 2019, but the timeframe now is to report to Council in early September, 2019. This will allow for the additional consultation Council has requested.

Also as directed, an option has been developed with no continuous centre turn lane, no loss of trees and no expropriation of property. The option does include continuous on road bike lanes while maintaining and minimizing impervious surfaces. It maintains the existing sidewalk on the south side of the corridor.

6. Protected Bike Lane Options

The Town indicated that Council also asked that options for protected bike lanes be assessed and four types of facility were evaluated:

- On-Road Bike Lanes with Fixed Curb/Delineator



To be presented/discussed:

ACTION BY:

- Dual Multi-Use Paths (No On-Road Bike Lanes)
- Two-Way Cycle-Track
- On-Road Bike Lanes with Painted Buffer

MC Reviewed the evaluation criteria and the pros and cons for the 4 options. The preferred option - On-Road Bike Lanes with Painted Buffer was carried forward for all the roadway alternatives being re-assessed.

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MC described the key principles that were applied in carrying out the re-assessment of the design alternatives. They include recognition of the scenic corridor, having continuous sidewalk/multi-use path and on road bike lanes, intersection improvements for traffic flow and safety, and urbanize to curb and gutter to improve stormwater management.

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MC outlined the approach taken for the development of the alternative designs being presented for discussion. The intent is to try to achieve a balance by including intersection improvements, continuous on road bike lanes, continuous and accessible sidewalks, and a multi-use path for all users, while respecting the scenic nature of the corridor, and minimizing impacts to trees and property.

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- Fourth Line to Dorval Drive – 1.5 km

He noted that is important when looking at the impacts associated with the design alternatives to keep in mind the overall length of the road segments being assessed.

10. Block 1 – Mississaga Street to East Street Evaluation Summary



To be presented/discussed:

ACTION BY:

For the block from Mississaga Street to East Street the preferred design as presented last year has retained the same cross section with three lanes of traffic, including the centre left turn lane, on road bike lanes with painted Buffer, and sidewalks on both sides.

Syed Rizvi (SR) indicated that consideration has been given to the Bronte Village land use plans and development proposals, including the redevelopment of the Bronte Village Mall. Design improvements for this block include the re-design of the Bronte Road intersection with Lakeshore Road West to reduce the skew and shorten the crosswalk lengths. Although it was noted that the positioning of buildings and driveway access, limits space and flexibility to achieve an ideal perpendicular alignment.

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MC also noted that some property will need to be acquired, primarily between Bronte Road and East Street, with small portions of frontages needed, mostly at intersections to accommodate the design improvements.

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MC then introduced the re-assessment of design alternatives carried out for the 3 sections of roadway corridor from East Street to Dorval Drive. The Traffic Study was updated to further assess the need for a continuous left turn lane, given the degree of impact to trees and property. It was determined that while there may be some safety improvements, overall the improvements to traffic flow were minor. Accordingly, for this portion of the corridor, the 3 lane design alternative with the centre left turn lane was not carried forward.

Alternative Design A – No Impact

MC described Alternative Design A, the No Impact design, which consists of two 3.30m traffic lanes, continuous on road bike lanes with the painted buffer, and the existing sidewalk on the south side. It includes the conversion



To be presented/discussed:

ACTION BY:

to curb and gutter. This alternative does not include a multi-use path, and the gaps in sidewalk on the north side remain.

Alternative Design A1 – Minimal Impact

This Alternative was developed from the No Impact design to fill in the gaps on the north side to provide a continuous sidewalk from East Street to Dorval Drive.

Alternative Design B – Hybrid

Alternative Design B carries forward the two 3.30m traffic lanes, and continuous on road bike lanes with the painted buffer, and includes the conversion to curb and gutter. With the alternative the existing sidewalk on the south side is converted to a multi-use path, and the gaps are filled on the north side to provide a continuous sidewalk from East Street to Dorval Drive.

Don Cox (DC) said it was his understanding that the assumed traffic volumes rise slightly to the year 2031 and then level out or start to drop. Jill Stephen (JS) noted that Wood carried out a review of the base Traffic Study to focus on Road Safety and Collision History, and to consider the warrant for Centre Left Turn Lane. With respect to anticipated traffic volumes, the modelling does indicate a levelling off of traffic growth and slight decline after 20131. These trends were based on assumptions about land use, intensification, and a shift in modal split to transit and active transportation.

There was discussion about the installation of the Multi-use Path (MUP) under the Hybrid Alternative. A continuous MUP was identified as a desirable improvement as it provides for broader active transportation activity by a variety of users, provides for those with accessibility needs, and attracts recreational cyclists who feel less safe using on-road bike lanes. It is the case that the wider width of the MUP in relation to the existing sidewalk will impact on trees, with predicted removals based on the high-level design footprint.

Pamela Knight (PK) said that the character of the area is such that there is not much recreational use. Given the pinch points along the corridor and the need to preserve the corridor, that it is acceptable to not install the MUP, to have discontinuous sidewalk, and require that pedestrians cross over.

DC said that retaining the Scenic Corridor is a top priority.



To be presented/discussed:

ACTION BY:

12. Block 2 Evaluation – East Street to Third Line

MC presented the impact evaluation table for the 3 alternatives for the Block from East Street to Third Line. It was noted that the tree removals in this and other sections come mostly as the result adding sidewalk and the multi-use path. MC noted that the Staff Recommendation for the Block 2 section from East Street to Third Line is for Alternative B, the Hybrid Design.

JS noted that the re-assessment as part of the EA study is still at a conceptual design level, not to the level of detail to tender for construction. At the detail design stage various mitigation measures can be applied to reduce impacts, and retain more trees.

13. Block 3 Evaluation – Third Line to Fourth Line

MC presented the impact evaluation table for the 3 alternatives for the Block from Third Line to Fourth Line. It was noted that there is a reduction in the impervious surface for both the No Impact and Minimal Impact Alternatives due to the fact that there are paved shoulders that would be removed with the transition to a 2 lane roadway with curb and gutter.

The Staff recommendation for the Block 3 section from Third Line to Fourth Line is for Alternative B, the Hybrid Design.

There was further discussion on the need for the MUP in this section of the corridor, given the impacts to trees. Neal Smith (NS) described the design standards that are applied to support the safe and accessible use of the path by pedestrians and cyclists. He noted that the meandering of the MUP, and narrowing from the standard 3m width to a 2.4m width, are a couple of the measures to apply at detail design to reduce impacts.

14. Block 4 Evaluation – Fourth Line to Dorval

MC presented the impact evaluation table for the 3 alternatives for the Block from Fourth Line to Dorval Drive. It was noted that the tree removals documented for Alternative B – the Hybrid Option are resulting from the installation of the Multi-use Path on the south side of the roadway, as well as the intersection improvements.

The Staff recommendation for the Block 3 section from Fourth Line to Dorval is for Alternative A1, the Minimal Impact options that includes continuous



To be presented/discussed:

ACTION BY:

sidewalks on both sides of the roadway, and intersection improvements at Morden Avenue, and Dorval Drive.

██████████ asked about the timing for implementation once the EA Study is complete. JS said that the staging plan as set out in the Town's Capital Forecast right now has a start in 2020 from Dorval Drive moving west to Mississaga Street. She noted that the normal timeline is 1 year for detail design, 1 year for utilities work, and 1 year for construction. The work will be done in stages to limit construction disruption for residents, and keep traffic moving.

15. General Comments

██████████ wanted to know when she would know what would happen to the Maple tree on her property. JS noted again that the Town's Capital Forecast presents the timelines for design and construction. It was noted that the sooner the design details are known, this provides time for residents to plan for and take action to address impacts.

RC spoke to the potential impacts when the storm sewers are installed. NS said that these impacts, along with those for utilities, signs etc. are all part of detailed design.

MC spoke about next steps, saying that this was the second of three meetings with stakeholders to discuss the corridor segments. Smaller "Kitchen Table" meetings are an option to take a look at site specific issues. These meetings are useful for Staff to know what the key concerns are, and what information is of importance to present to the public.

The intent now is to have a meeting with the public sometime around the end of May. JS said that following the Public Meeting, Staff will work with the Consultant to finalize the Environmental Study Report (ESR) that documents the planning and design process, and the public input received. A recommendation report will then be taken to Council, likely in September, with a request to file a Notice of Completion of the EA Study, and make the ESR available for the mandatory public review period.

PK requested a copy of the presentation that she could share with the Coronation Park Resident's Association Members. She said that they prefer the public meeting format over the open house. It was confirmed that the meeting would be similar to the last one held in July, with a presentation, and

Town



Minutes

Date: Monday, April 1, 2019, 2 pm start **Meeting at:** Oakville Town Hall, Trafalgar Room

Ref: TPB166147.1802 Lakeshore Road Improvements EA

Subject/purpose:

Stakeholder Meeting #2 – Corridor Section Fourth Line to Dorval Drive

Attendees:

Councillor Chisholm, Ward 2	██████████, Lakeshore Road Resident
Councillor Duddeck, Ward 2	██████████, Lakeshore Road Resident
Jill Stephen, Town of Oakville	Jewell Kennedy, Appleby College
Mark Covert, Town of Oakville	Clark Euale, Appleby College
Syed Rizvi, Town of Oakville	Innes Van Nostrand, Appleby College
Jill MacInnes, Town of Oakville	██████████, Lakeshore Road Resident
Neal Smith, Wood	██████████, Lakeshore Road Resident
Bob Felker, Wood	██████████, Lakeshore Road Resident
	██████████ Holyrood Avenue Resident
	██████████, Holyrood Avenue Resident

To be presented/discussed:

ACTION BY:

1. Opening Remarks

Mark Covert (MC) with the Town of Oakville (Town) welcomed everyone to the session to discuss progress with the Lakeshore Road West Improvements Class EA Study.

Introductions were made. It was noted that this is the third of 3 meetings with Stakeholders. the presentation material is the same, but the invited attendees and the focus is on three Corridor Blocks with this third meeting looking at the road section from Fourth Line to Dorval Drive.

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3. EA Process

The EA Process was outlined and it was stressed that in reflecting what was heard at the Public Meeting held in July last year that we take a step back and re-assess the proposed design. It is recognized that safety and capacity are major concerns. The focus of the re-assessment is on the section of the Lakeshore Road West corridor from East Street to Dorval Drive.

The reassessment identified and evaluated alternative designs, and a preferred design is being proposed that has been altered from that previously presented.

4. Existing Conditions

Images showing existing road conditions were presented. Features and conditions of note included the scenic nature of the corridor, the poor pavement condition, gaps in sidewalks and on-road bike lanes, and the open drainage swales.

5. Council Direction

The resolution passed by Town Council at its August 7, 2018 meeting was reviewed. The intent was to report back to Council in early 2019, but the timeframe now is to report to Council in early September, 2019. This will allow for the additional consultation Council has requested.

Also as directed, an option has been developed with no continuous centre turn lane, no loss of trees and no expropriation of property. The option does include continuous on road bike lanes while maintaining and minimizing impervious surfaces. It maintains the existing sidewalk on the south side of the corridor.



6. Protected Bike Lane Options

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MC reviewed the evaluation criteria and the pros and cons for the 4 options. The preferred option - On-Road Bike Lanes with Painted Buffer was carried forward for all the roadway alternatives being reassessed.

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MC described the key principles that were applied in carrying out the re-assessment of the design alternatives. They include recognition of the scenic corridor, having continuous sidewalk/multi-use path and on road bike lanes, intersection improvements for traffic flow and safety, and urbanize to curb and gutter to improve stormwater management.

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- Fourth Line to Dorval Drive – 1.5 km



He noted that is important when looking at the impacts associated with the design alternatives to keep in mind the overall length of the road segments being assessed.

10. Block 1 – Mississaga Street to East Street Evaluation Summary

For the block from Mississaga Street to East Street the preferred design as presented last year has retained the same cross section with three lanes of traffic, including a centre left turn lane, on road bike lanes with painted buffer, and sidewalks on both sides.

Syed Rizvi (SR) indicated that consideration has been given to the Bronte Village land use plans and development proposals, including the redevelopment of the Bronte Village Mall. Design improvements for this block include the redesign of the Bronte Road intersection with Lakeshore Road West to reduce the skew and shorten the crosswalk lengths, although it was noted that the positioning of buildings and driveway accesses limit space and flexibility to achieve an ideal perpendicular alignment.

MC described the impacts associated with the preferred design for the Mississaga Street to East Street corridor block. There will be some tree removals, mostly in the road section from Mississaga Street to Bronte Road. There will be some tree removals in the Bronte Village section to East Street, but tree removals will be replaced with trees in pits (Silva Cell), as part of a broader streetscaping plan.

In response to a question from Councillor Duddeck (CD), MC also noted that some property will need to be acquired, primarily between Bronte Road and East Street, with small portions of frontages needed, mostly at intersections to accommodate the design improvements.

11. Design Alternatives - Blocks 2, 3 & 4

MC then introduced the reassessment of design alternatives carried out for the 3 sections of roadway corridor from East Street to Dorval Drive. The Traffic Study was updated to further assess the need for a continuous left turn lane, given the degree of impact to trees and property. It was determined that while there may be some safety improvements, overall the improvements to traffic flow were minor. Accordingly, for this portion of the corridor, the 3 lane design alternative with the centre left turn lane was not carried forward.



Alternative Design A – No Impact

MC described Alternative Design A, the “No Impact Design,” which consists of two 3.30m traffic lanes, continuous on road bike lanes with the painted buffer, and the existing sidewalk on the south side. It includes the conversion to curb and gutter. This alternative does not include a multi-use path, and the gaps in sidewalk on the north side remain.

Alternative Design A1 – Minimal Impact

This alternative was developed from the No Impact Design to fill in the gaps on the north side to provide a continuous sidewalk from East Street to Dorval Drive.

Alternative Design B – Hybrid

Alternative Design B carries forward the two 3.30m traffic lanes, and continuous on road bike lanes with the painted buffer, and includes the conversion to curb and gutter. With this alternative the existing sidewalk on the south side is converted to a multi-use path, and the gaps are filled on the north side to provide a continuous sidewalk from East Street to Dorval Drive.

There was discussion about the state of repair of the existing sidewalks. Jewell Kennedy (JK) asked if cracked sidewalk sections would be replaced. MC responded that maintenance to Town standards is ongoing. The design criteria for the sidewalks calls for standard width concrete sidewalk repair or to fill in gaps on the north side of the corridor.

12. Block 2 Evaluation – East Street to Third Line

MC presented the impact evaluation table for the 3 alternatives for the block from East Street to Third Line. It was noted that the tree removals in this and other sections come mostly as the result of adding sidewalk and the multi-use path. MC noted that the staff recommendation for the Block 2 section from East Street to Third Line is for Alternative B, the Hybrid Design.

JS noted that the reassessment as part of the EA study is still at a conceptual design level, not to the level of detail to tender for construction. At the detail design stage various mitigation measures can be applied to reduce impacts, and retain more trees.



13. Block 3 Evaluation – Third Line to Fourth Line

MC presented the impact evaluation table for the 3 alternatives for the Block from Third Line to Fourth Line. It was noted that there is a reduction in the impervious surface for both the No Impact and Minimal Impact Alternatives due to the fact that there are paved shoulders that would be removed with the transition to a 2-lane roadway with curb and gutter.

The staff recommendation for the Block 3 section from Third Line to Fourth Line is for Alternative B, the Hybrid Design.

14. Block 4 Evaluation – Fourth Line to Dorval

MC presented the impact evaluation table for the 3 alternatives for the block from Fourth Line to Dorval Drive. It was noted that the tree removals documented for Alternative B – the Hybrid Option are resulting from the installation of the multi-use path on the south side of the roadway, as well as the intersection improvements.

The staff recommendation for the Block 3 section from Fourth Line to Dorval is for Alternative A1, the Minimal Impact option that includes continuous sidewalks on both sides of the roadway, and intersection improvements at Morden Avenue and Dorval Drive.

CD made a number of points about actions taken by Town staff to address concerns raised by residents about the impact of proposed improvements to the corridor from Fourth Line to Dorval Drive. It was noted the Town staff had met with some residents on site to observe property conditions first hand. It was also noted that some of the impacts are tied to intersection improvements that come in response to road user safety concerns.

Property-specific issues were raised related to elevation differences and grading requirements, potential impact to trees and retaining walls, and drainage concerns. A specific question was asked about the alignment and width of the new structure to cross McCraney Creek.

Wood

JS suggested that it would be worthwhile to set up meetings between residents and Appleby College, and Town staff and the consultant to observe existing conditions, and lay out the footprint of proposed improvements under the different design concepts.

Town



██████████ asked how much flexibility there is to adjust to road alignment to reduce impacts. Neal Smith (NS) responded that there is greater flexibility to meander the sidewalk and the MUP to avoid impacts to trees, but changes to the road alignment are more difficult as design standards need to be maintained, and a shift to reduce impacts on one side, may result in impacts on the other side of the roadway.

██████████ made the comment that the conversion to curb and gutter is desirable as right now as without the curb, drivers are passing on the shoulders.

15. General Comments

MC spoke about next steps, saying that this was the third of three meetings with stakeholders to discuss the corridor segments. Smaller “Kitchen Table” meetings are an option to take a look at site-specific issues. These meetings are useful for staff to know what the key concerns are, and what information is of importance to present to the public.

The intent now is to have a meeting with the public sometime around the end of May. JS said that following the Public Meeting, staff will work with the consultant to finalize the Environmental Study Report (ESR) that documents the planning and design process, and the public input received. A recommendation report will then be taken to Council, likely in September, with a request to file a Notice of Completion of the EA Study, and make the ESR available for the mandatory public review period.

Jill MacInnes (JM) said that ample notice will be given, and information will be made available online in advance of the meeting.

Councillor Chisholm (RC) made the point that one size does not fit all and that it is important to try to achieve a balance between meeting user needs and safety, and protecting the scenic nature of the corridor and minimizing impacts.

16. Adjourn

The meeting adjourned with thanks to the Attendees.



Meeting Minutes prepared by:

Wood Environment & Infrastructure Solutions
a Division of Wood Canada Limited

Per: Bob Felker

BF/

Attachments:

Presentation – Lakeshore Road West Improvements Stakeholder Meetings



To be presented/discussed:

ACTION BY:

a Q & A session, and then break-out opportunities would be set up to provide for one-on-one review of plans. RC noted that it will be important at the outset to set out that this is what we heard from you and here is how we have responded to your comments and concerns.

Jill MacInnes (JM) said that ample notice will be given, and information will be made available online in advance of the meeting. **Town**

16. Adjourn

The meeting adjourned with thanks to the Attendees.

Meeting Minutes prepared by:

Wood Environment & Infrastructure Solutions
a Division of Wood Canada Limited

Per: Bob Felker

BF/

Attachments:

Presentation – Lakeshore Road West Improvements Stakeholder Meetings



Memo

To: Syed Rizvi and Mark Covert
From: Neal Smith
Date: May 10, 2019
File: TPB166147
cc: David Sinke and Bob Felker, Wood
Re: **Lakeshore Road West - Resident Site Visits**

On Friday, May 3, 2019, site visits were scheduled to meet the residents and councillors at their residences to discuss their concerns regarding the roadway design and how it will impact their property and trees in front of their properties. The schedule was set to start at west end (Fourth Line) and end at the east end (Dorval Drive). Attendees from the Town of Oakville (Mark Covert and Syed Rizvi), Town Councillors (Cathy Duddeck and Ray Chisholm) and Wood (Neal Smith) attended. Below are details from each visit:

[REDACTED]
This property is on the north side and is the second property east of McCraney Creek. The following points of the meeting were discussed:

- A review of the trees in front of the property was completed;
- It was noted that that a couple of the trees were low value trees;
- A measurement of 5.8 metres was taken from center line of the road to the approximate north limit of the proposed back of curb. This included:
 - 3.3m west bound through lane;
 - 0.5m painted buffer for the bike lane;
 - 1.8m on-road bike lane (includes 0.3m gutter); and
 - 0.2m curb.
- I was noted that a 1.5m sidewalk would be placed within the boulevard space between the back of the proposed curb and the existing property line. The placement of the sidewalk would be the most appropriate location that had the least amount of conflict with trees, property, utilities etc.;
- Was noted that due to the recent confirmation of property from the Town on the south side of Lakeshore Road, there is a possibility that a shift of the centre line maybe possible, but would have to be confirmed at the detailed design phase;
- [REDACTED] discussed the grade of this driveway and how the water during a rain event runs through this property and if this would be dealt with during the road construction. Councillor Chisholm and Mark advised that this would be the owners responsibility as it is on private property. [REDACTED] would have to discuss with the contractor and make his own arrangements. Any connection to the Town's storm water system would have to be approved by the Town. It was also noted that with the installation of curb and gutter and catchbasins, this may alleviate the issue;



Town of Oakville
May 10, 2019

- [REDACTED] noted that the existing catch basin is always getting plugged up;
- The meeting ended and the everyone was pleased at outcomes; and
- Was noted that a PIC was planned for the end of the month.

[REDACTED]
This property is Appleby College and has its entrance on the south side of Lakeshore Road West adjacent Suffolk Avenue. The following points of meeting were discussed:

- [REDACTED] noted that eastbound traffic is very busy in the morning backing up to almost Fourth Line and westbound traffic is very busy in the afternoon;
- [REDACTED] noted that her only concern was what impacts are expected on the south side (Appleby College property);
- It was noted that where no major impacts based on the preliminary design. No impacts to trees are expected;
- There is a pedestrian crossing proposed on the west leg of the intersection. There is a very high number of pedestrians crossing on the west side of the intersection;
- The existing centre median island on the west leg will be reviewed for removal;
- [REDACTED] was pleased with the meeting and discussions held; and
- Was noted that a PIC was planned for the end of the month.

[REDACTED]
These properties are both on the north side of Lakeshore Road West and have gardens that encroach onto Town of Oakville right-of-way. The following points of meeting were discussed:

- A measurement of 5.8 metres was taken from center line of the road to the approximate north limit of the proposed back of curb. This included:
 - 3.3m west bound through lane;
 - 0.5m painted buffer for the bike lane;
 - 1.8m on-road bike lane (includes 0.3m gutter); and
 - 0.2m curb.
- I was noted that a 1.5m sidewalk would be placed within the boulevard space between the back of the proposed curb and the existing property line. The placement of the sidewalk would be the most appropriate location that had the least amount of conflict with trees, property, utilities etc.;
- It was explained to the property owners that the gardens encroach on to public lands. The agreed and understood;
- [REDACTED] explained that as much as he understands his garden encroaches into the right-of-way there is wasted space on the south side that could be used. He requested the Town look at shifting the centre line of the road to the south to avoid/lessen the impacts to the beautiful garden and trees;
- Much discussion took place with an agreement that the Town and Wood would review the possibly of shifting the centre line of the roadway south to lessen the impacts to the trees and the gardens on the northside;
- Was noted that a PIC was planned for the end of the month.

Town of Oakville
May 10, 2019

[REDACTED]
This property is on the north side of Lakeshore Road West. The following points of meeting were discussed:

- It was noted his property is well setback and the large garden and trees are within the Town's right-of-way;
- A measurement of 5.8 metres was taken from center line of the road to the approximate north limit of the proposed back of curb. This included:
 - 3.3m west bound through lane;
 - 0.5m painted buffer for the bike lane;
 - 1.8m on-road bike lane (includes 0.3m gutter); and
 - 0.2m curb.
- I was noted that the existing 1.5m sidewalk would likely remain where it is currently, and a small boulevard would likely be placed between the back of curb and the sidewalk. Any refinements would be completed during the detailed design phase;
- [REDACTED] explained he has drainage issues and asked if this could be dealt with during construction. Councillor Chisholm and Mark advised that this would be the owners responsibility as it is on private property. [REDACTED] would have to discuss with the contractor and make his own arrangements. Any connection to the Town's storm water system would have to be approved by the Town. It was also noted that with the installation of curb and gutter and catchbasins, this may alleviate some of the issue;
- Was noted that a PIC was planned for the end of the month.

[REDACTED]
These properties are both corner lots on the southside of Lakeshore Road West, [REDACTED] on the southeast corner and [REDACTED] on the northwest corner. The following points of meeting were discussed:

- A measurement of 5.8 metres was taken from center line of the road to the approximate north limit of the proposed back of curb. This included:
 - 3.3m west bound through lane;
 - 0.5m painted buffer for the bike lane;
 - 1.8m on-road bike lane (includes 0.3m gutter); and
 - 0.2m curb.
- I was noted that the existing 1.5m sidewalk would likely remain where it is currently and a small boulevard would likely be placed between the back of curb and the sidewalk. Any refinements would be completed during the detailed design phase;
- It was noted that the owners of [REDACTED] had purchased property from the Town, extending their property to the north. This is to be confirmed;
- As there were no noted impacts related to either property, Councillor Duddeck agreed to wait for the owners to arrive and provide them the details; and
- Councillor Duddeck indicated to the residents were the back of curb would be and advised that no changes are expected that would impact trees or property.

Town of Oakville

May 10, 2019

Wood and Town follow items to be discussed in a conference call:

- Mark, Syed and Neal discussed the need to possibly take the design to higher level of detail now to confirm the shift to the south from Forth Line to Whittington Place is possible and will fit within the limits of the right-of-way. Will this impact the new structure? What will it look like? What about the grade difference on the south side? Will a wall be required?
- Town and Wood would review the possibility of shifting the centre line of the roadway south to lessen the impacts to the trees and the gardens on the northside at 461 and 465 Lakeshore Road West;
- Mark discussed the what format we should use for presenting the design to the public. No red circles indicating tree removals.
- Neal to setup a conference with the Town.

**Wood Environment & Infrastructure Solutions
A Division of Wood Canada Limited**

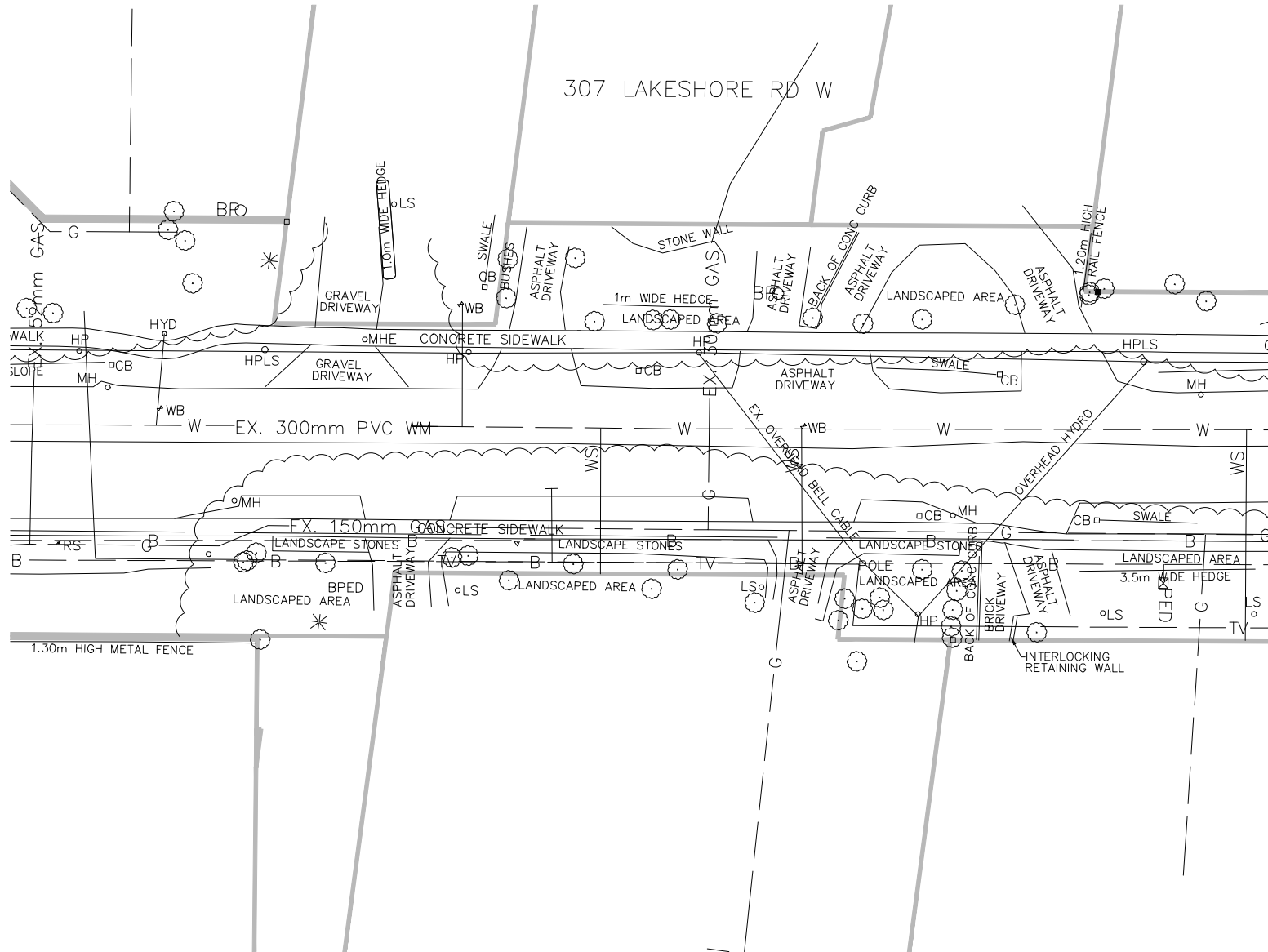


Neal Smith, C.E.T

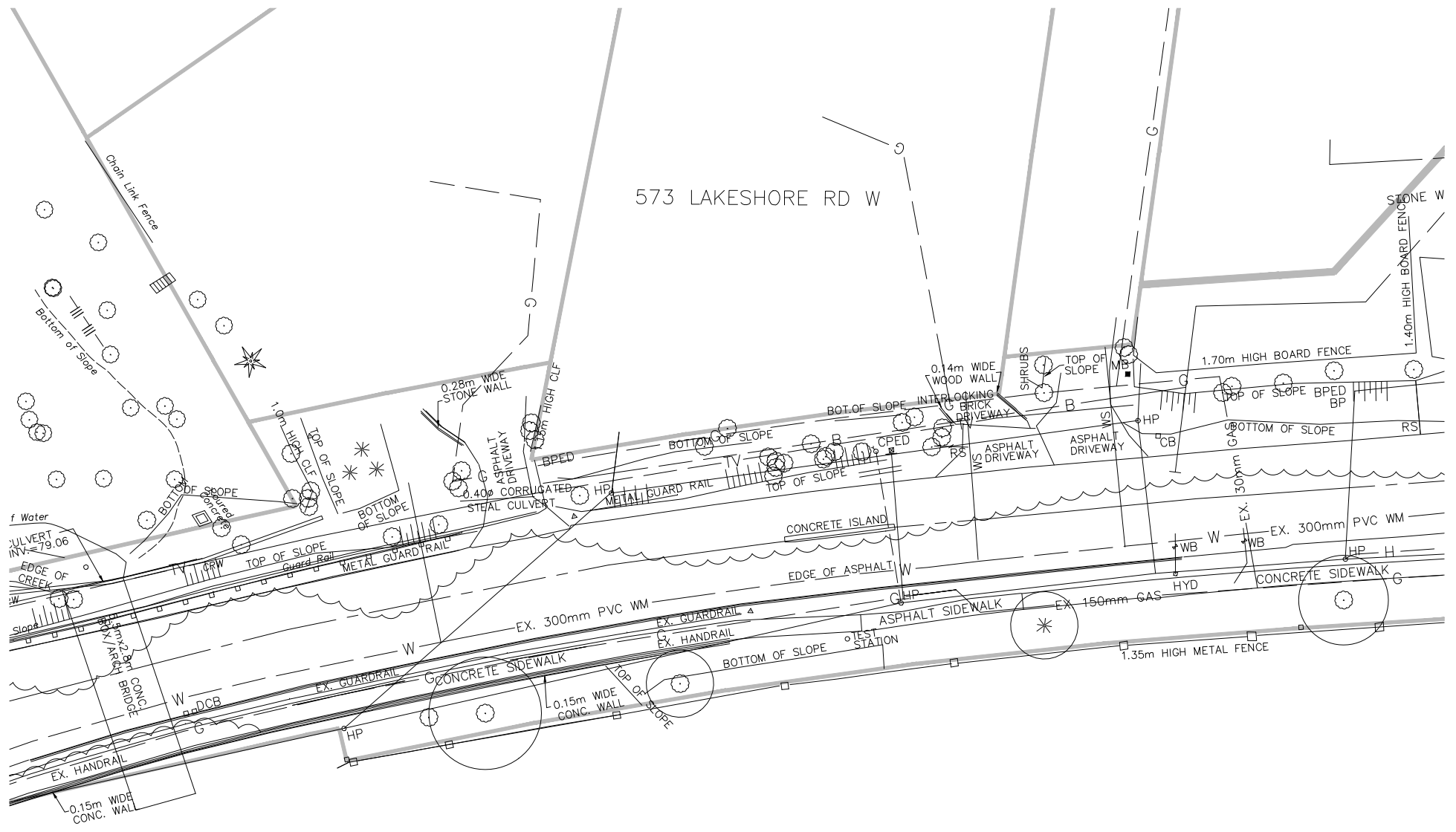
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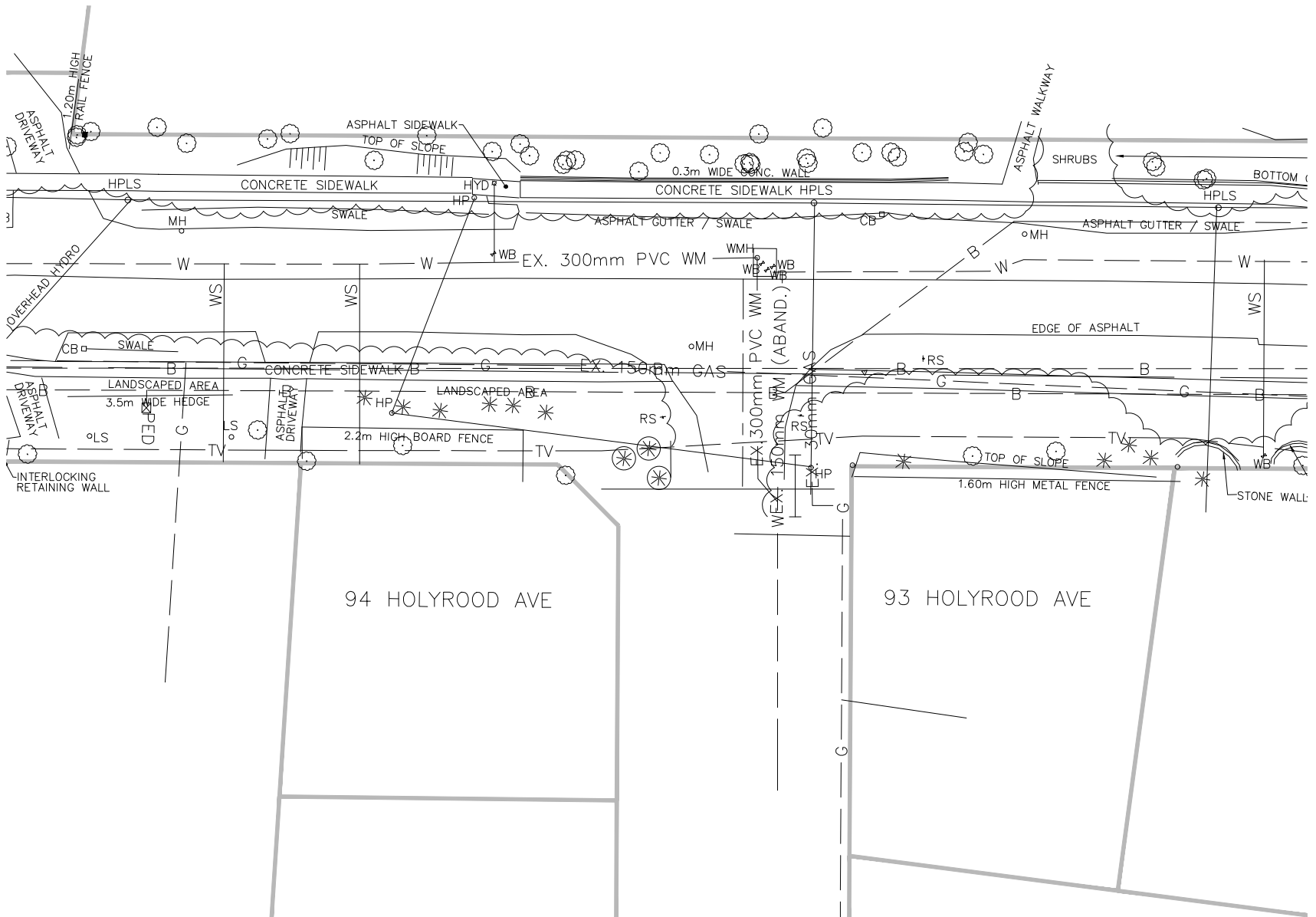


307 LAKESHORE RD W



573 LAKESHORE RD W





Mcandrew, Louise

From: Smith, Neal
Sent: May-06-19 6:21 PM
To: Felker, Bob; Sinke, David
Cc: Mcandrew, Louise
Subject: Re: Thanks !

Thanks Bob. I will put a memo together, that's a good idea while it's fresh in my mind.

Thanks
Neal

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From: Felker, Bob <bob.felker@woodplc.com>
Sent: Monday, May 6, 2019 4:51 PM
To: Sinke, David; Smith, Neal
Cc: Mcandrew, Louise
Subject: RE: Thanks !

Hi Neal, glad the meeting(s) went well. May be helpful to have a summary Memo to File of observations/comments made by the respective property owners. We should have something to put in the public record to append to the ESR, even if it is just Councillor Duddeck's email.

Bob
Bob Felker
O: 519-650-7139
M: 226-751-3854

From: Sinke, David
Sent: May-04-19 4:37 PM
To: Smith, Neal <neal.smith@woodplc.com>; Felker, Bob <bob.felker@woodplc.com>
Subject: RE: Thanks !

Great, good to see positive comments. Thanks Neal.

From: Smith, Neal
Sent: Saturday, May 04, 2019 4:22 PM
To: Sinke, David <david.sinke@woodplc.com>; Felker, Bob <bob.felker@woodplc.com>
Subject: Fwd: Thanks !

FYI

Get [Outlook for iOS](#)

From: Cathy Duddeck <cathy.duddeck@oakville.ca>

Sent: Saturday, May 4, 2019 2:40 PM

To: [REDACTED]
[REDACTED]

Cc: Syed Rizvi; Smith, Neal; Mark Covert; Jill Stephen; Ray Chisholm

Subject: Thanks !

Just a quick note from Ray and I to thank everyone for taking the time to meet with us yesterday in regards to the aspects of the Lakeshore Road West improvements and the potential effect on your properties.

I believe the presence of staff and the consultant was invaluable to answer specific concerns you might have... as they mentioned yesterday there will a plan shared with everyone once they have had the opportunity to drill down into the details of the project.

Enjoy your weekend !

Cathy & Ray

Cathy Duddeck

Ward 2 Town & Regional Councillor

Office of the Mayor & Council

Town of Oakville | 905-845-8374 | www.oakville.ca

Vision: To be the most livable town in Canada

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Talpur, Mir

From: Syed Rizvi <syed.rizvi@oakville.ca>
Sent: Monday, February 22, 2021 4:41 PM
To: Cathy Duddeck; Ray Chisholm; Sean O'Meara; Beth Robertson
Cc: Jill Stephen; Martin Maguire; Jill MacInnes; Sinke, David; Felker, Bob; Christina Tizzard; Talpur, Mir; Mcandrew, Louise
Subject: Lakeshore Road EA - Stakeholders Meeting
Attachments: Lakeshore Road West EA - Public Notice Update January 2021.pdf; Stakeholders List.docx

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Good afternoon Councillors,

Following public feedback and Council direction, the study team has been reassessing the Lakeshore Road West roadway design concepts to ensure that the elements identified in the Scenic Corridor Study are incorporated into the final technically preferred design.

Our next step is to meet with area stakeholders to get their feedback. As March break has been rescheduled, the suggested stakeholders' meeting date is:

- **Tuesday, March 16, 11 am - noon**

Please let me know if this time is acceptable to you.

Attached is our current list of stakeholders to which we will mail the invite. Please let me know if you have anyone you wish to add to the list by the end of this week, so that we can get invitations out early next week.

Also attached for your reference is a copy of the public update notice sent to the residents and stakeholders last month.

If you have any questions in the meantime, feel free to call or email me.

Thank you,
Syed

Syed Rizvi, M.Sc., P. Eng
Transportation Engineer
Transportation and Engineering
Town of Oakville | 905-845-6601, ext.3981 | www.oakville.ca

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From: Syed Rizvi
Sent: January 25, 2021 12:37 PM

To: Mayor Rob Burton <Mayor@oakville.ca>; Cathy Duddeck <cathy.duddeck@oakville.ca>; Ray Chisholm <ray.chisholm@oakville.ca>; Sean O'Meara <sean.o'meara@oakville.ca>; Beth Robertson <beth.robertson@oakville.ca>
Cc: Jill Stephen <jill.stephen@oakville.ca>; Martin Maguire <martin.maguire@oakville.ca>
Subject: Lakeshore Road West Improvements EA Update

Good afternoon Mayor Burton and Councillors,

Please find attached a Lakeshore Road West Improvements EA project update letter being sent to area residents and project stakeholders in the next few days.

A copy of the update notice is attached for your reference.

Thanks,
Syed

January, 2021

Lakeshore Road West Improvements January Update

Dear Resident:

We want to give you an update on the work completed to date on the Lakeshore Road West Improvements Study and what to expect next.

The town is conducting a Lakeshore Road West Improvements Study to assess the current and future transportation conditions along Lakeshore Road West from Mississaga Street to Dorval Drive. The town has identified the need for improvements to cycling, pedestrian and vehicular facilities within the study area to meet the town's vision and growth demands.

In August, 2019, the Lakeshore Road study was paused while the town completed a Scenic Corridor study to further examine the study area to identify and evaluate the roadway and street scape elements that contribute to its scenic value.

The Scenic Corridor study was completed in February 2020 and identifies following four themes that support and define the characteristics and qualities of the scenic corridor:

- Importance of maintaining trees and vegetation
- Importance of maintaining and complementing the historic attributes
- Importance of maintaining connectivity for pedestrians and cyclists
- Importance of maintaining the proportions of the existing roadway

The study team is reviewing the revised design concepts for Lakeshore Road West to ensure these elements are incorporated into the final technically preferred design.

What's next

Public consultation is a key feature of this study and we hope to have revised draft plans to share with you soon. We'll reach out again to advise you of a future public meeting where you can share your thoughts and input.

In the meantime, you can find background information on the Lakeshore Road West Study page at oakville.ca.

For more information, or if you have any questions or comments, please contact ServiceOakville at 905-845-6601 or service@oakville.ca.

Talpur, Mir

Subject: Lakeshore Road West Improvement Environmental Study Project - Stakeholders Meeting- Tuesday March 16, 11 am
Location: <https://zoom.us/j/98453507487>
Start: Tue 3/16/2021 11:00 AM
End: Tue 3/16/2021 12:00 PM
Recurrence: (none)
Meeting Status: Accepted
Organizer: Syed Rizvi

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

-----Original Appointment-----

From: Syed Rizvi <syed.rizvi@oakville.ca>

Sent: Tuesday, March 2, 2021 4:53 PM

To: Syed Rizvi; Cathy Duddeck; Ray Chisholm; Sean O'Meara; Beth Robertson; Jill Stephen; Martin Maguire; Christina Tizzard; Sinke, David; Felker, Bob; Jill MacInnes

Subject: Lakeshore Road West Improvement Environmental Study Project - Stakeholders Meeting- Tuesday March 16, 11 am

When: Tuesday, March 16, 2021 11:00 AM-12:00 PM (UTC-05:00) Eastern Time (US & Canada).

Where: <https://zoom.us/j/98453507487>

CAUTION: External email. Please do not click on links/attachments unless you know the content is genuine and safe.

Good afternoon,

This calendar invite is for the Lakeshore Road West stakeholders meeting on March 16, 11 am.

To join the Zoom Meeting on March 16, click on the link or dial in using one of the numbers below:

<https://zoom.us/j/98453507487>

Meeting ID: 984 5350 7487

One tap mobile

+16473744685,,98453507487# Canada

+16475580588,,98453507487# Canada

Here is how to join the meeting by phone:

Dial by your location

+1 647 374 4685 Canada

+1 647 558 0588 Canada

+1 778 907 2071 Canada

+1 204 272 7920 Canada

+1 438 809 7799 Canada

+1 587 328 1099 Canada

Meeting ID: 984 5350 7487

Find your local number: <https://zoom.us/j/98453507487>

If you have any questions in the meantime, feel free to call or email me.

Thank you,
Syed

Municipal Class Environmental Assessment for Lakeshore Road West Improvements (Mississaga Street to Dorval Drive)

Study Update - Presentation of the Preferred Design
Stakeholder Meeting - March 16, 2021



WITHIN REACH

A simple manoeuvre can spare cyclists from serious injury.

HERE'S HOW TO DO THE DUTCH REACH



1 Check your side mirror for cyclists. If you'd like a reminder to do that, ask for a free **Watch for Bikes** mirror decal from CAA.

2 Reach toward the door with your hand furthest from it and, as you pivot, check your blindspot for cyclists coming up behind you.

3 If you spot a bike, freeze. Wait until they pass and look to see if there are any more coming.

4 Once the coast is clear, slowly open the door and exit your vehicle.

CAA MAGAZINE | SUMMER 2020

For more information, visit: <https://www.dutchreach.org/>

Agenda

1. Introductions and Safety Moment
2. Overview and Purpose of this study
3. Class EA Process / Study Recap
4. Key Comments Received during the EA process
5. Scenic Corridors Study
6. Study Area and Corridor Blocks
7. Preferred Design
8. Stormwater Management
9. Potential Impacts and Mitigation Measures
10. Next Steps

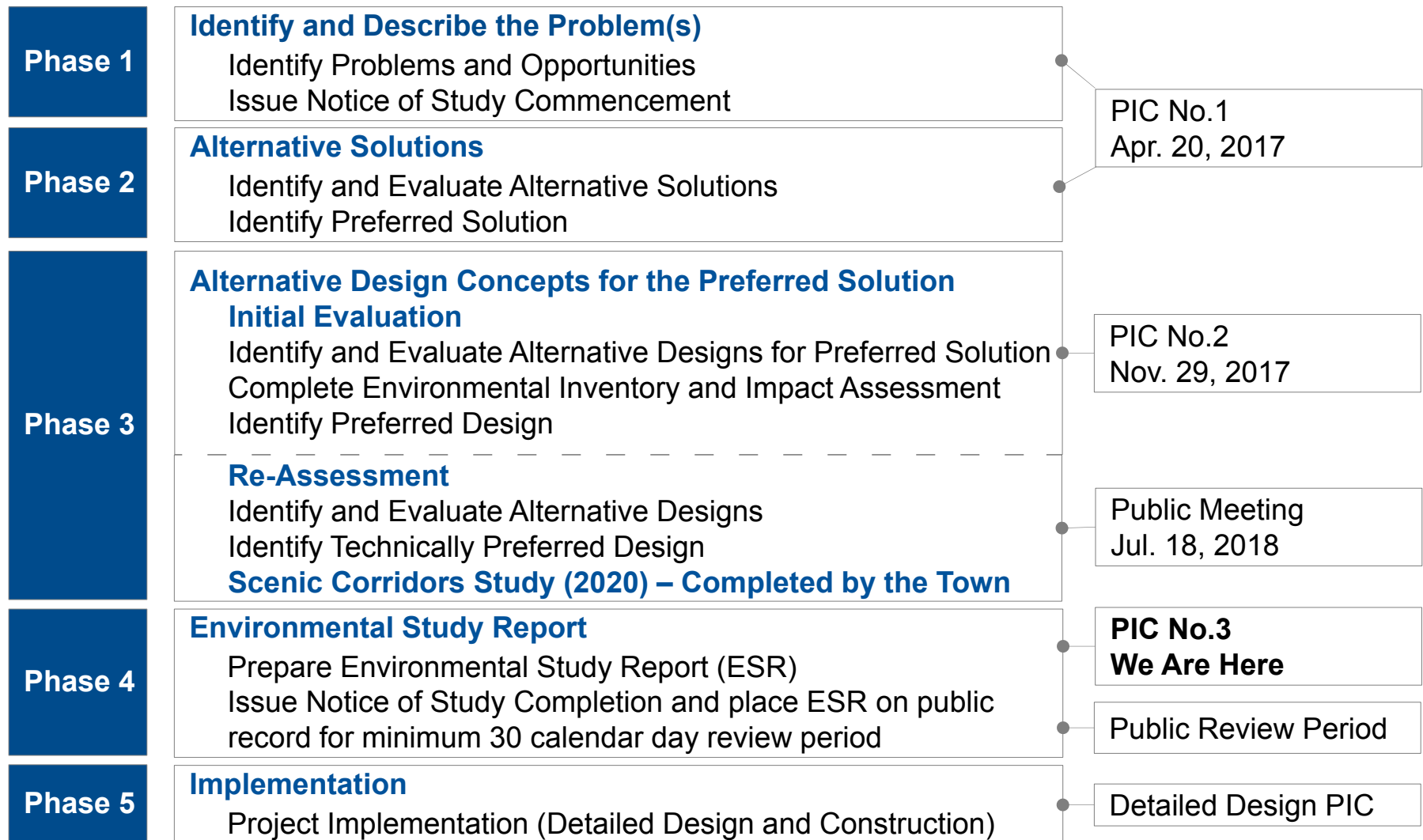
Overview and Purpose of this study

The Town of Oakville issued the Notice of Commencement for this study in January 2017. The Study is being completed following the Municipal Class Environmental Assessment process for a Schedule 'C' project, as outlined in the Municipal Class Environmental Assessment document.

The purpose of this study is to identify improvements to Lakeshore Road West (Mississauga Street to Dorval Drive) to address the following:

- Pedestrian and cyclist safety and accessibility
- Traffic safety
- Pavement conditions
- Drainage and storm water management
- Intersection safety and operations
- Roadway design to meet current standards
- Creek crossings and structures

Class EA Process / Study Recap



Key Comments Received





Throughout the EA Study, following common themes/concerns were received by the Study Team:

- Loss of trees
- Purchase of strip of land across frontage
- Increase in impervious area
- Provision of infrastructure for pedestrian and bicycle users and space for safety
- Provision of pedestrian crossings

Based on extensive consultation and direction from the Town Council, revised alternative design concepts were introduced and evaluated to identify a preferred design that has minimal impacts on the concerns noted above. In addition, a Scenic Corridors Study was completed by the Town staff to identify and evaluate the roadway, streetscape and framing elements that contribute to the scenic value.

Scenic Corridors Study

The **Scenic Corridors Study** was completed in February 2020. It identified the following four themes:

Importance of maintaining trees and vegetation	
Importance of maintaining and complementing the historical attributes	
Importance of maintaining connectivity for pedestrians and cyclists	
Importance of maintaining the proportions of the existing roadway	

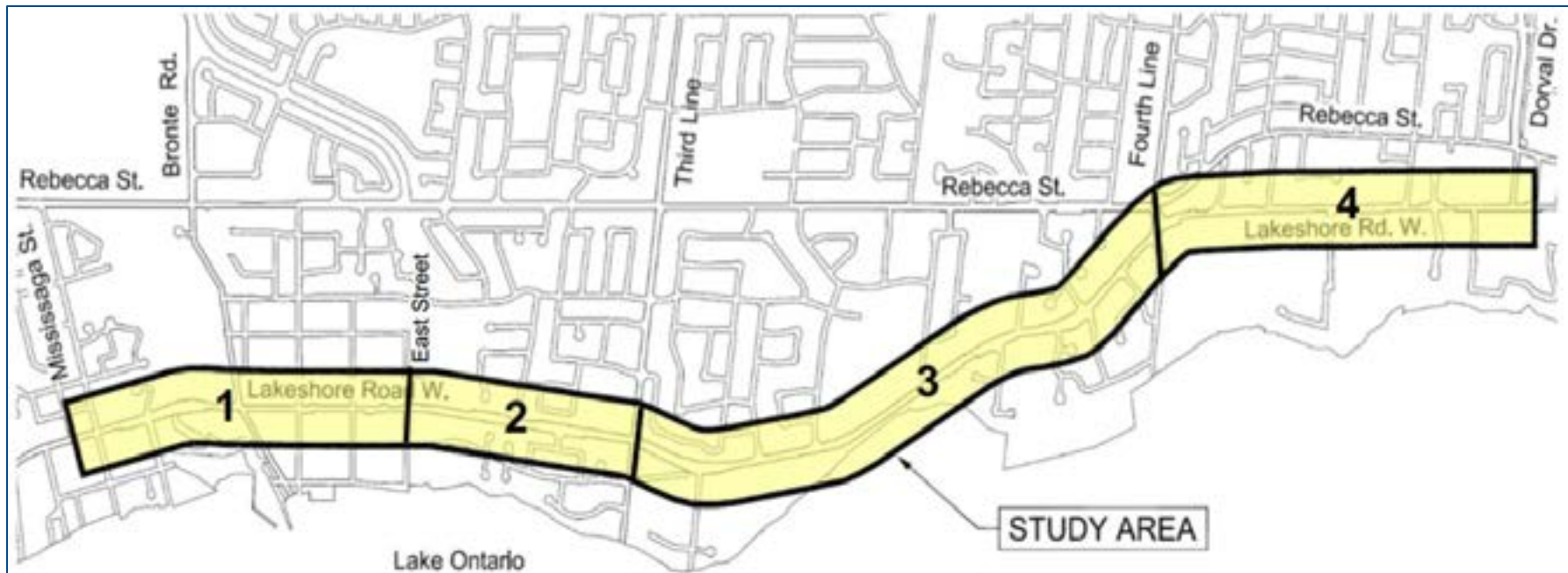
The Preferred Design was reviewed against these themes to assess degree of satisfaction and opportunities for improvement.

Scenic Corridors Study Report is available on Town of Oakville's website: <https://www.oakville.ca/business/scenic-corridors.html>

Study Corridor Blocks

The Study Area was divided into following four blocks:

- Block 1 - Mississaga Street to East Street;
- Block 2 - East Street to Third Line;
- Block 3 - Third Line to Fourth Line; and
- Block 4 - Fourth Line to Dorval Drive.



Preferred Design – Block 1

The key design elements of preferred design for **Study Corridor Block 1 (Mississaga Street to East Street)** include the following:

- 3.3 metre through lanes, with a 3.5 metre centre turn lane
- 2.0 metre sidewalks and 1.5 metre on road bike lanes with a 0.5m painted buffer in both directions
- Intersection improvements at Bronte Road



Photo 1 - Existing Conditions



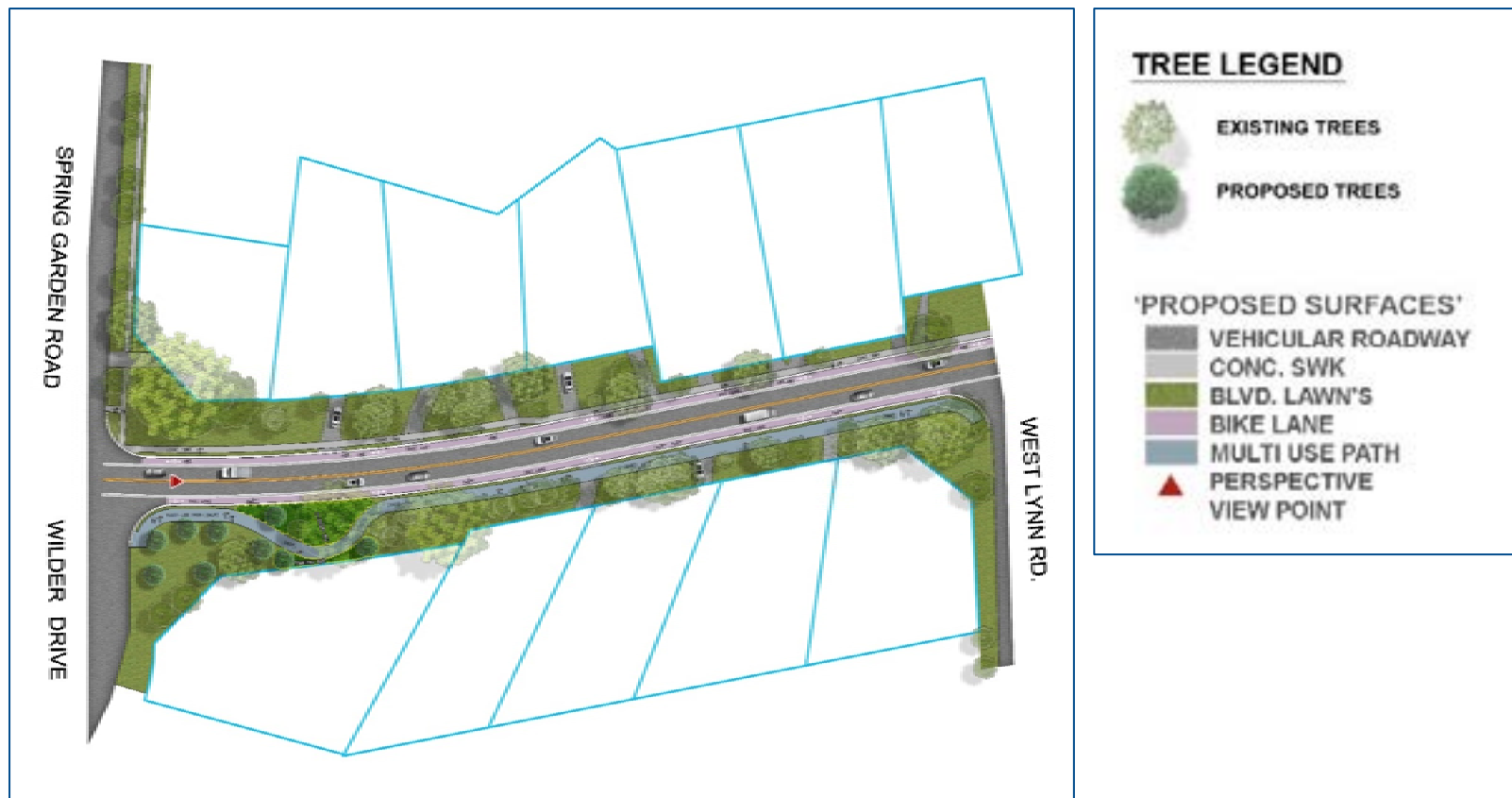
Proposed Cross-Section – Block 1

The proposed cross-section for **Study Corridor Block 1 (Mississaga Street to East Street)** is shown below:



Conceptual Plan View – Blocks 2 and 3

Conceptual plan view rendering showing configuration of future Lakeshore Road West (within Blocks 2 and 3) is provided below:



Conceptual Rendering – Blocks 2 and 3

Conceptual rendering showing configuration of future Lakeshore Road West (within Blocks 2 and 3) is provided below:



Preferred Design – Block 2

The key design elements of preferred design for **Study Corridor Block 2 (East Street to Third Line)** include the following:

- 3.3 metre through lanes (no centre turn lane)
- 1.8 metre on road bike lanes with a 0.5m painted buffer
- 1.5 metre continuous sidewalk on the north side and 3.0 metre continuous multi-use path on south side
- Convert to urban-standard curb & gutter cross-section with storm sewer system
- Intersection improvements

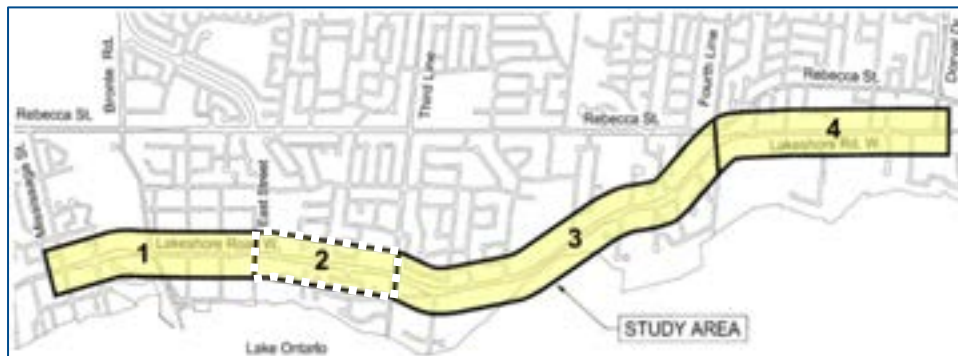
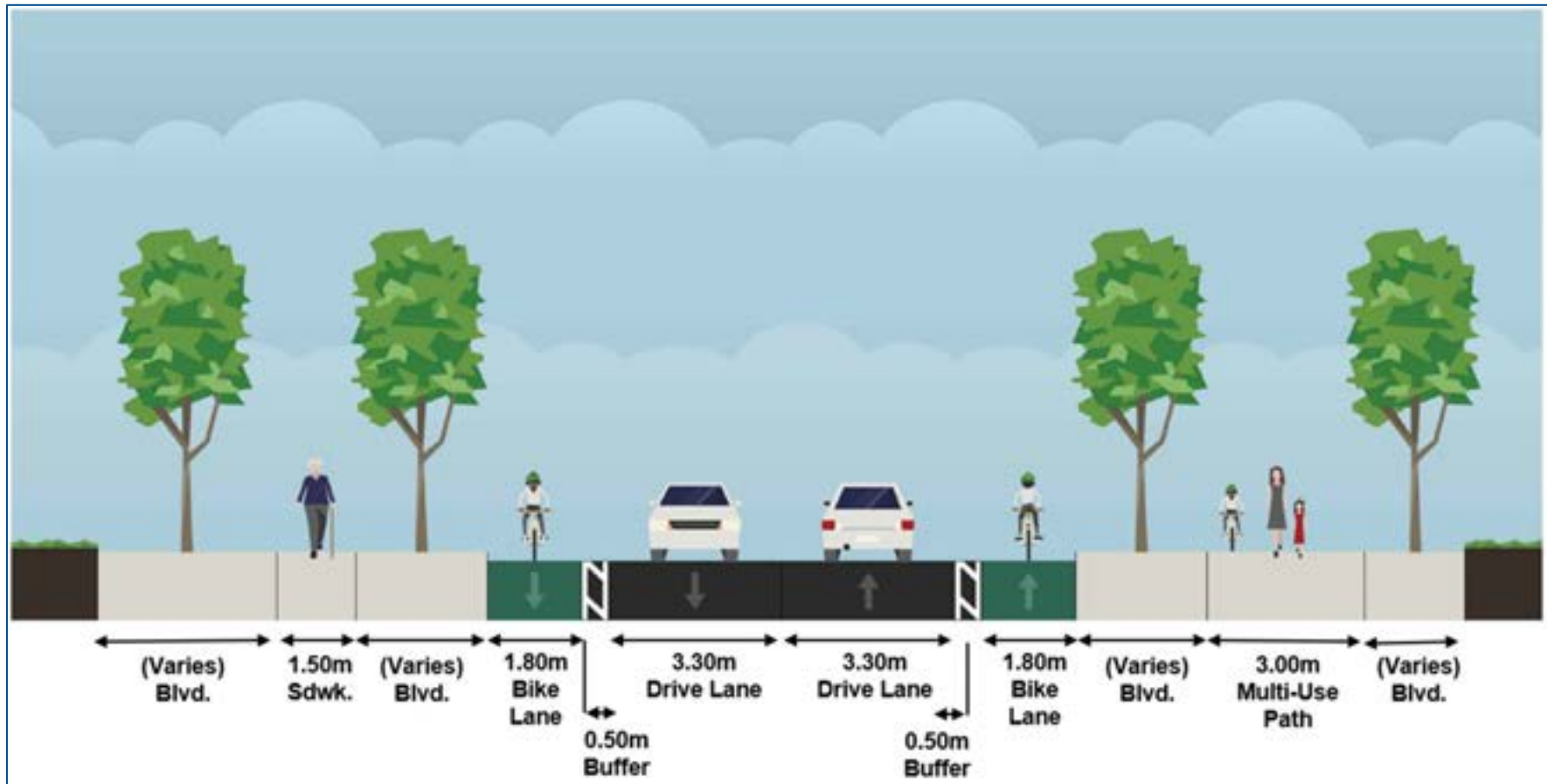


Photo 2 - Existing Conditions



Proposed Cross-Section – Block 2

The proposed cross-section for **Study Corridor Block 2 (East Street to Third Line)** is shown below:



Preferred Design – Block 3

The key design elements of preferred design for **Study Corridor Block 3 (Third Line to Fourth Line)** include the following:

- 3.3 metre through lanes (no centre turn lane)
- 1.8 metre on road bike lanes with a 0.5m painted buffer
- 1.5 metre continuous sidewalk on the north side and 3.0 metre continuous multi-use path on south side
- Convert to urban-standard curb & gutter cross-section with storm sewer system
- Intersection improvements

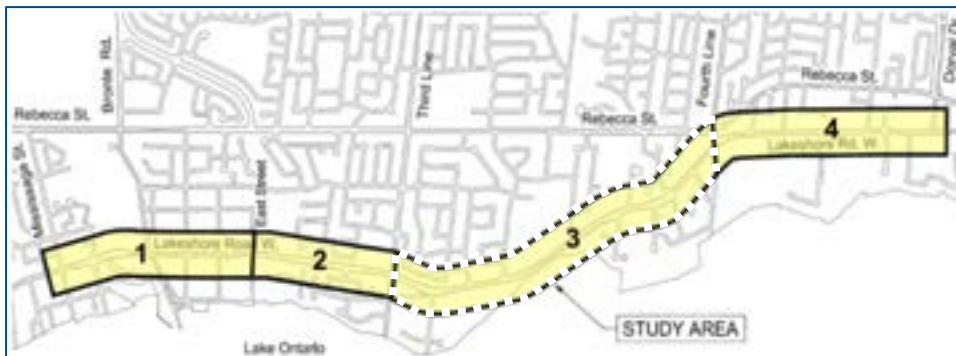
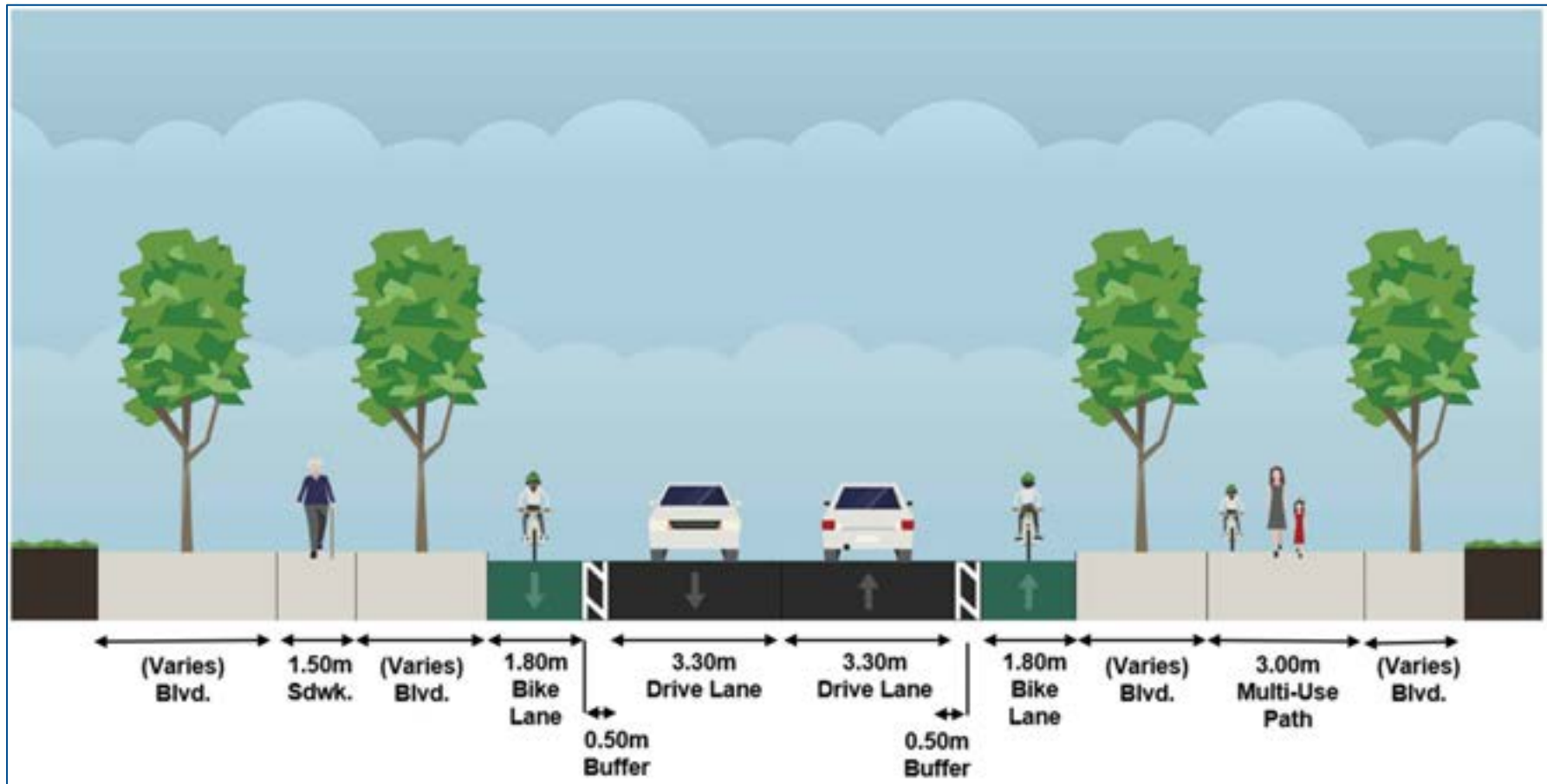


Photo 3 - Existing Conditions



Proposed Cross-Section – Block 3

The proposed cross-section for **Study Corridor Block 3 (Third Line to Fourth Line)** is shown below:



Conceptual Rendering – Block 4

Conceptual rendering showing configuration of future Lakeshore Road West (within Block 4) is provided below:



Preferred Design – Block 4

The key design elements of preferred design for **Block 4 (Fourth Line to Dorval Drive)** include the following:

- 3.3 metre through lanes (no centre turn lane)
- 1.8 metre on road bike lanes with a 0.5m painted buffer
- Convert to urban-standard curb & gutter cross-section with storm sewer system
- 1.5m sidewalks (where missing)
- Intersection improvements

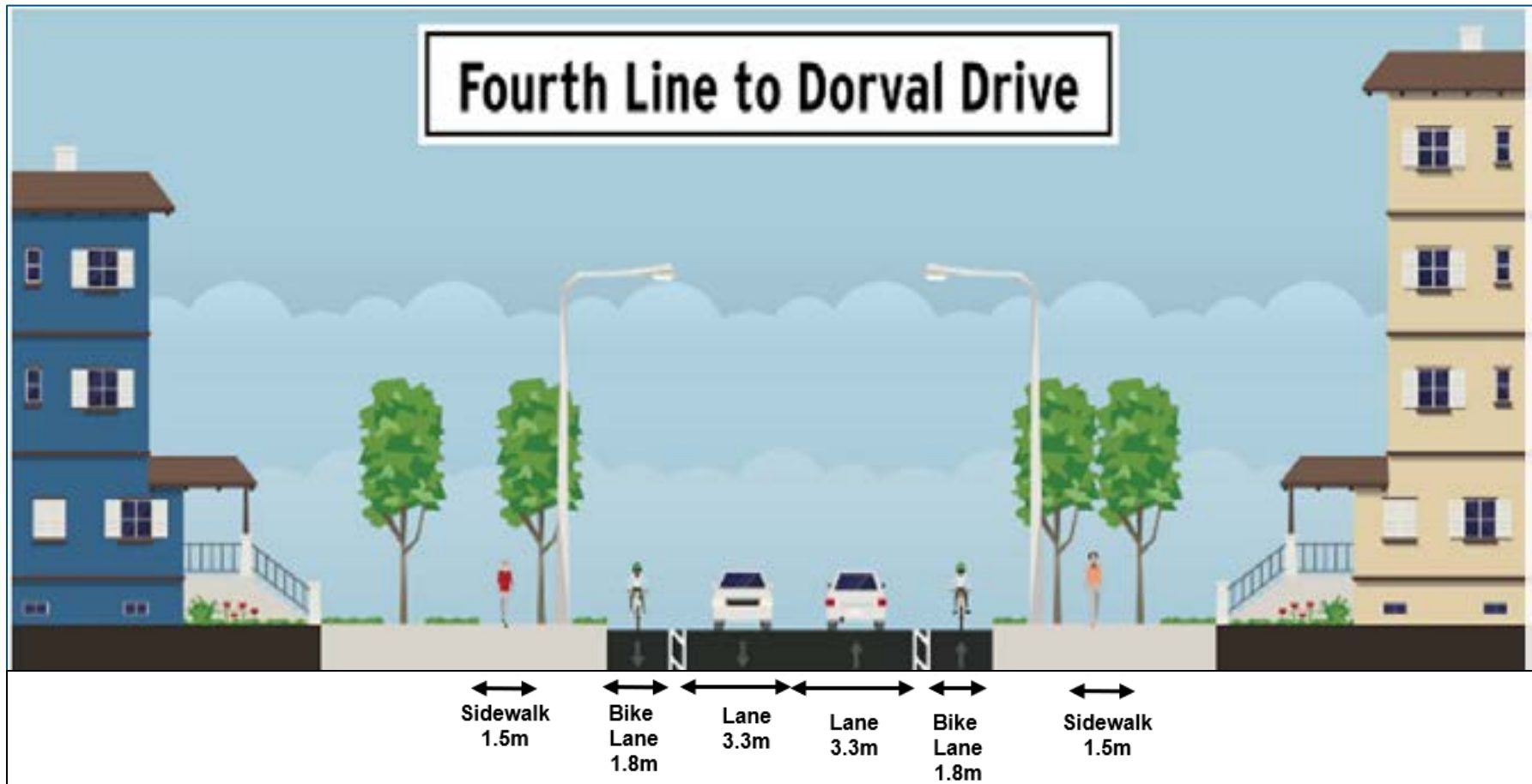


Photo 4 - Existing Conditions



Proposed Cross-Section – Block 4

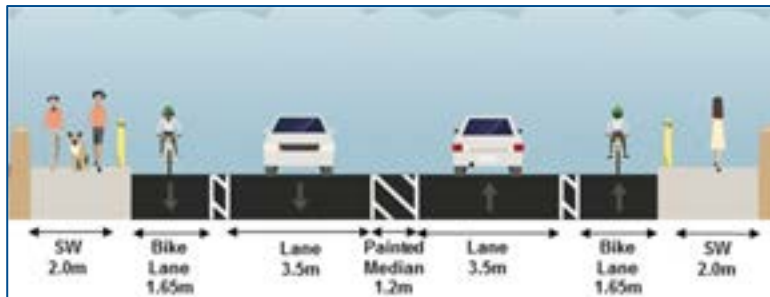
The proposed cross-section for **Study Corridor Block 4 (Fourth Line to Dorval Drive)** is shown below:



Bronte Creek and 14 Mile Creek Bridges

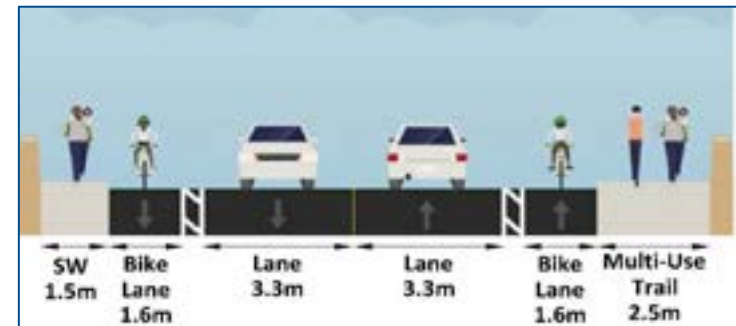
Bronte Creek Bridge

The existing Bronte Creek bridge requires no structural changes. It will be modified with a new pavement marking plan to accommodate the new cross-section. Pedestrian protection will also be provided by incorporating a pedestrian separation barrier wall or railing.



14 Mile Creek Bridge

The existing 14 Mile Creek bridge requires no structural changes. The bridge deck will be modified with new pavement markings to accommodate the new cross-section.



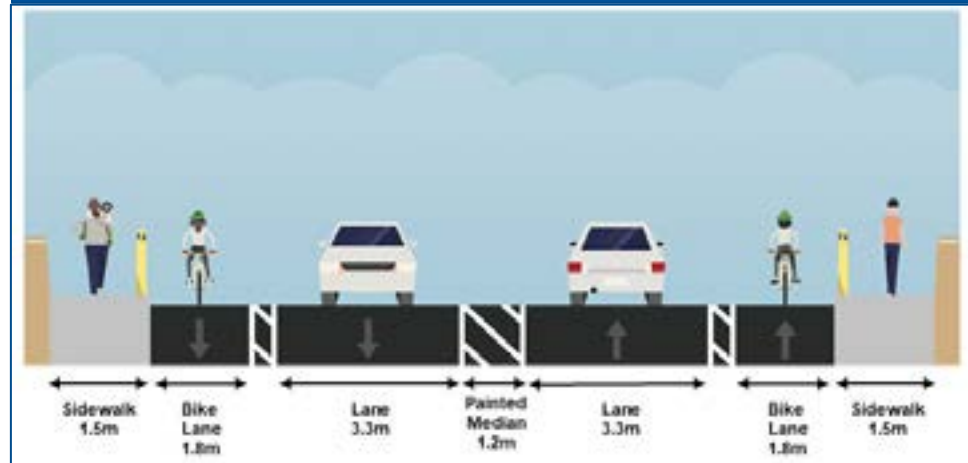
Preferred Design - McCraney Creek Bridge

The McCraney Creek Bridge requires replacement due to poor condition and flooding potential. The new structure will accommodate two-lanes of traffic, on-road bike lanes and sidewalk (north and south sides). Pedestrian protection will also be provided by incorporating a pedestrian separation barrier wall or railing.

McCraney Creek – Existing Conditions



McCraney Creek Bridge – Proposed Cross-Section



Stormwater Management

The proposed road improvements will increase impervious coverage within various sections of the the Lakeshore Road right-of-way. The road will be a fully urbanized (i.e., curb and gutter on both sides).

A **Stormwater Management Report** was completed by Wood in 2021 to propose measures to manage stormwater. This report was completed in accordance with the recommendations of the Town of Oakville's Stormwater Master Plan (2020). This recently approved plan includes policy guidance requiring Low Impact Development (LID) Best Management Practice (BMP) source controls in both the private and public realm to provide 25 mm of runoff capture.

The **Stormwater Management Report** recommended various LID BMP measures to provide source control (infiltration) to offset the hydraulic impacts of land use intensification and climate change.

Low Impact Development BMP - Examples

Enhanced Grass Swale



Soil Cell Cross Section



Permeable Pavement

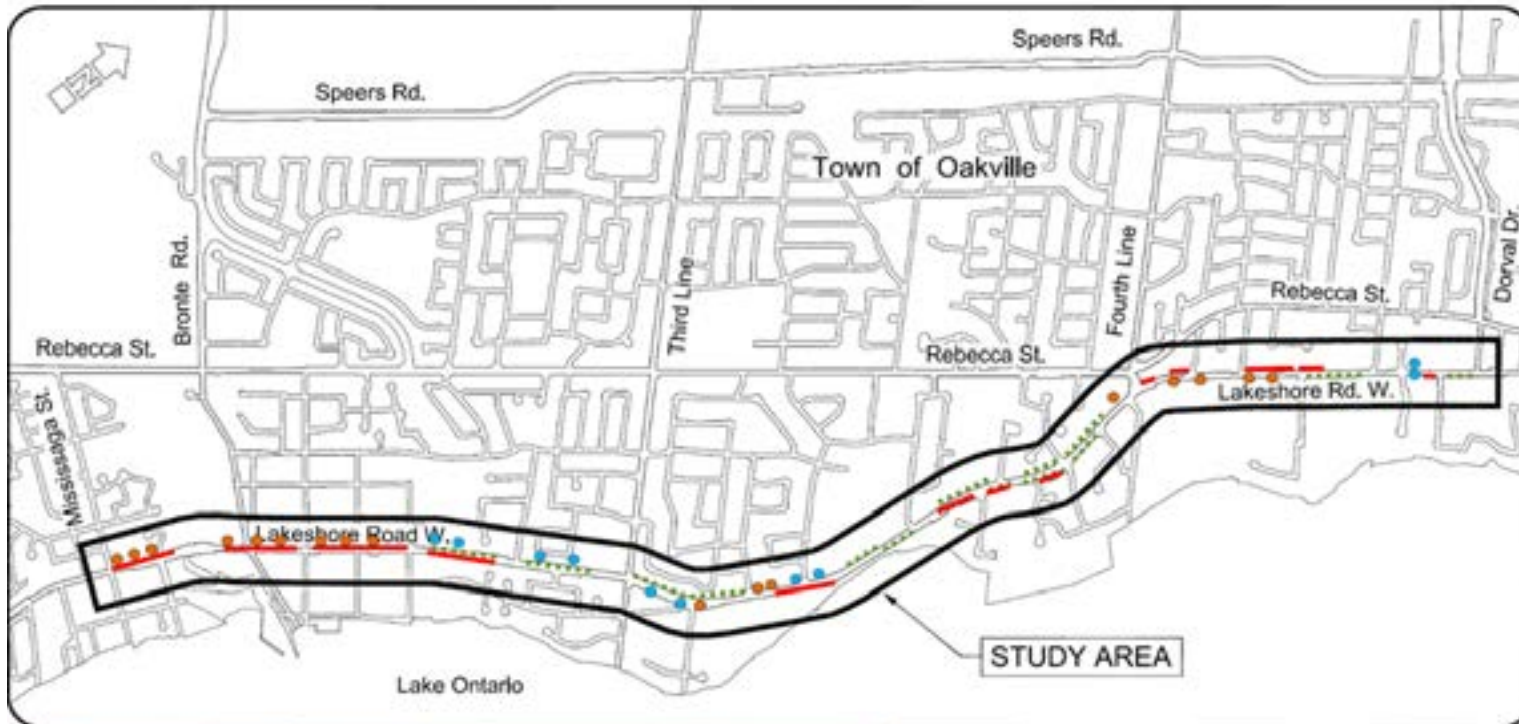


Bioretention Facility



Low Impact Development BMP - Locations

LID BMPs are recommended at the following 26 locations:



Recommended LID BMPs

- Enhanced Grass Swales
- Bioretention Systems
- Permeable Pavers/Pavement or Infiltration Trenches
- Silva Cells

Potential Impacts and Mitigation Measures (1 of 3)



Land Use

Future land use within the Study Area will remain consistent with the current uses of commercial and residential, with increasing infill.



Noise

Construction related temporary noise impacts are anticipated. The contractor will be responsible for controlling noise, in adherence with the Town of Oakville Noise By-Law 2008-098.



Aquatic Resources

In-water works should occur within appropriate timing windows for construction. The potential for erosion risk due to runoff from construction activities is low, if appropriate mitigation and environmental protection measures are applied. Further mitigation measures to protect aquatic habitat will be developed during the detailed design phase.

Potential Impacts and Mitigation Measures (2 of 3)



Terrestrial Resources

Minor impacts to wildlife and supporting habitat are anticipated from the proposed project works. Confirmation of habitat use within identified Significant Wildlife Habitats will be conducted at the detailed design stage.



Tree Preservation

The technically preferred design was selected following the re-evaluation of alternatives that considered impacts to mature trees and other vegetation. The existing trees located along the Lakeshore Road corridor are preserved, where possible. Where tree removals are required, these trees will be replaced. A Tree Replacement Plan will be developed during the detailed design phase. The Tree Replacement Plan will recommend native trees and vegetation and identify areas for their planting.

Potential Impacts and Mitigation Measures (3 of 3)



Archaeological Resources

A Stage 1 archaeological assessment has determined that a potential for archaeological resources exists within the Study Area. A Stage 2 archaeological assessment is required prior to any form of land alteration within the areas of archaeological potential.



Cultural Heritage Resources

In order to avoid impacts, cultural heritage features will be depicted on project drawings during detailed design phase and appropriate notes will be included that state that impacts on these features should be avoided.



Property Requirements

Narrow property strips will be required along frontages of land to implement sidewalk where gaps exist.

Next Steps



The Study Team will host an Online Public Engagement Event to present conclusions of this Study. The public will be informed about this event (format and timing) via a notice published in the local newspaper.



The Study Team will circulate the draft Environmental Study Report to the government agencies (e.g., Ministry of the Environment, Conservation and Parks; Conservation Halton).



The Town of Oakville staff will present the staff report to the Council for approval in May 2021.



With Council Approval, the Study Team will finalize the Environmental Study Report and issue it for a minimum of 30-day public review period. If no Part II Order Requests are received, the project will proceed to implementation.

Lakeshore Road West Improvements

April 22, 2021 at 10:00 am

Attendees:

Syed Rizvi
Bob Felker
Mir Talpur

Town of Oakville
Wood E&IS
Wood E&IS

Summary of Discussion:

These two properties are located on the north side of Lakeshore Road West, east of Suffolk Avenue. These properties have gardens that encroach onto Town of Oakville right-of-way. The following points of meeting were discussed:

- Zaid said that his comments are a restatement of what he said in his email shared with Syed 20 April 2021
- He noted that the impacts to the landscaping in front of his property will narrow down the entrance to the property. It will be difficult and less safe to access the property with the narrower driveway.
- The landscaping currently serves as a retaining wall and there is a concern that regrading to accommodate the road improvements will create steeper driveways and
- Eastbound Lakeshore Road jogs around Appleby College and is closer to the two properties.
- Bryan is concerned about damage to the existing landscaping, and has calculated that there is 6m of room to shift the road south.
- Syed noted that the design at this stage is at a preliminary level. Field survey will be required to determine the exact locations for the proposed road improvements (e.g. bike lanes, sidewalks, etc.). The design will be refined during the detailed design, and the public will have the opportunity to review and comment on the detailed design plans.
- He also said that the consultation will be part of the detailed design process.
- Bob added that since 2018, the design has been revised to remove third lane. One of the objectives of the study was to provide continuous pedestrian facilities throughout the Study Corridor. Majority of the grading line is on the Town property, and a very small portion is on 461 and 465 Lakeshore Road properties.
- He also pointed to a number of constraints that will need to be addressed in order to refine the road alignment in this area to avoid impacts on the gardens in front of 461 and 465 Lakeshore Road properties. For example, shifting the alignment to the south may trigger impacts to the other properties. There is a hydro line along the south of Lakeshore Road. Appropriate clearance between the edge of the road and hydro poles will be required. The existing sidewalk may also have to be removed and replaced if the hydro line is shifted south.

- And it was noted that alignment cannot be shifted in front of an individual property or two. It will have to be adjusted such that various design criteria are met. This will trigger alignment refinement over a larger stretch, instead of just one or two residences.
- Bryan asked if the alignment is not adjusted and impacts to the landscaping are identified, would the Town be responsible for finances associated with impacts to private landscaping, driveways, etc.?
- Bob noted that when the detailed design is developed, the restoration of the driveways, boulevards, sidewalks etc. within the town ROW and adjacent private property in accordance with town policies.
- Bryan inquired what is the planned timing for construction?
- Syed noted that on approval of the Environmental Assessment study, the project will proceed to detailed design and utility relocation before the construction can start. It is anticipated that the construction may commence in year 2023. The proposed staging as presented in the virtual PIC is to commence with Block 4 in the west from Dorval to Fourth Line, and move eastward in stages.
- Bob added that the road surface in some areas is reaching their service life. Also, the McCraney Creek Bridge has structural deficiencies that necessitate replacement as part of, or separate from the broader proposed improvements.
- Zaid highlighted that it's not just his and Bryan's property that are being impacted. There are also other properties between Suffolk Avenue and Morden Road will be impacted. If the alignment is straightened from Suffolk Avenue, it will remove the curve in the road in this section. Mr. Sahab also noted that he has measured the distance between hydro poles and edge of the road on some other roads. This distance seems to be minimal. For example, the distance between the hydro lines and the edge of road on Speers Road, is minimal (1-2 feet). There may be a good opportunity for the Town to correct the road alignment to straighten it between Suffolk Avenue and Morden Road.
- The call concluded with Syed and Bob thanking Zaid and Bryan for their time and the input. Their comments will be added to the public record, and we will continue communicate as this study is wrapped up and if and when the process moves to detailed design.