

# Public Realm Framework & Streetscape Plans Kerr Village

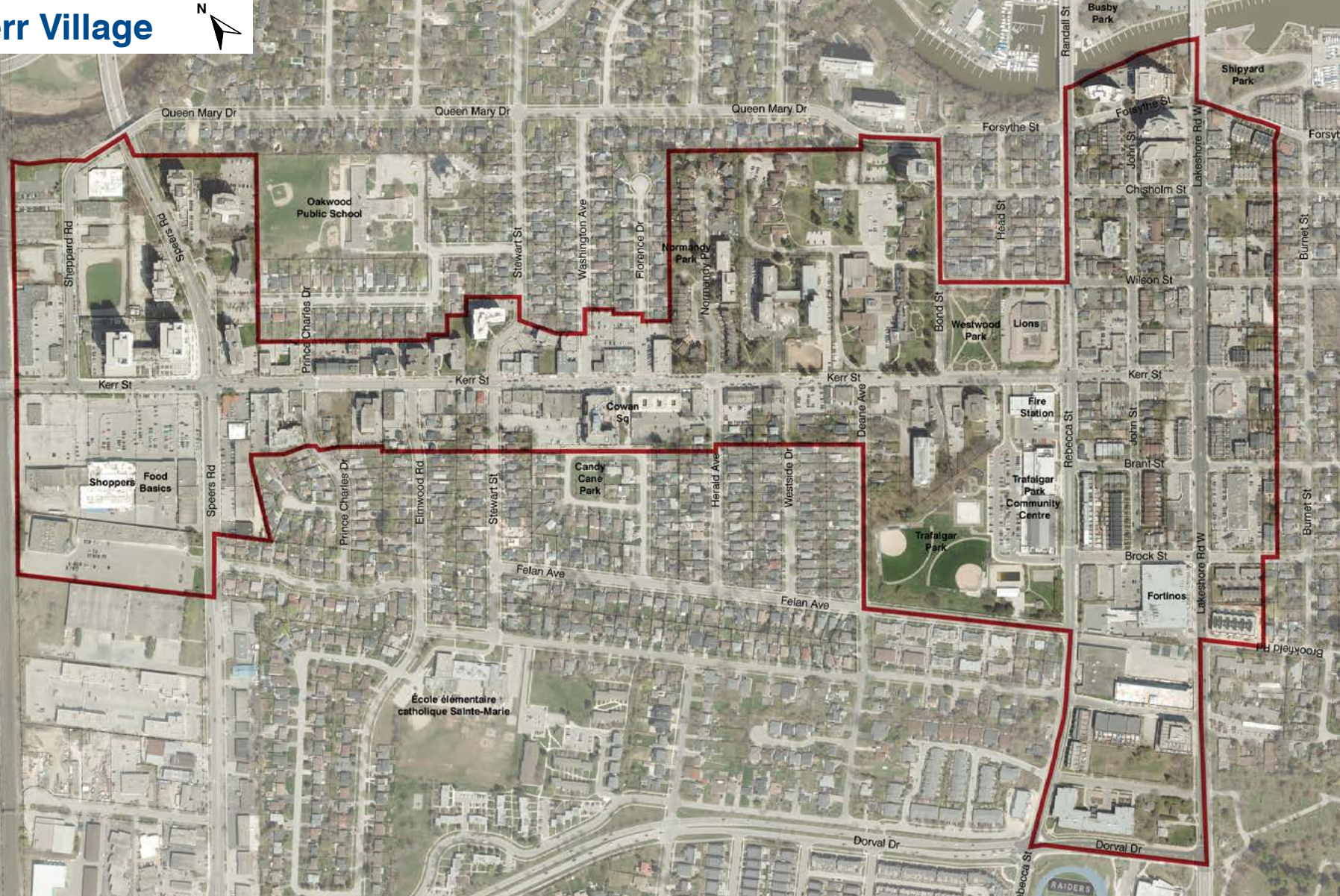
## Public Workshop #1

June 27, 2024

Bronte Village

Kerr Village







# What is a public realm study?

## Public Realm Study

Public realm refers to publicly accessible and highly visible spaces located along our streets, boulevards, pedestrian ways, parks, plazas and the interface with buildings.

The public realm study will identify existing amenities and opportunities for physical improvements to enhance the spaces and connections between the traditional commercial main street and surrounding community.



# What is a streetscape study?

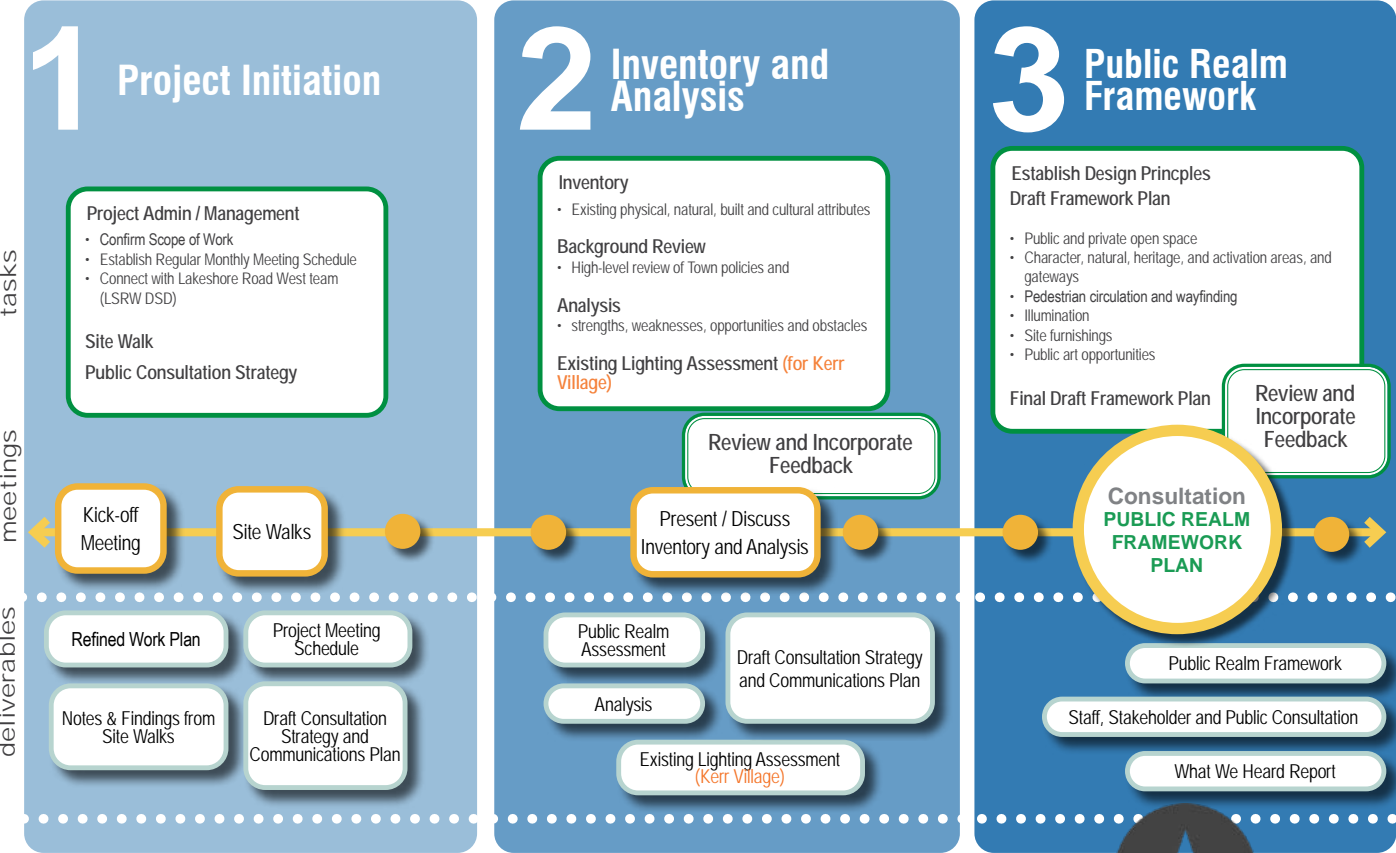
## Streetscape Plan

Streetscape refers to the physical environment of a street. This includes its visual character and how the space is used by pedestrians, patrons, cyclists and motorists.

The streetscape concept plans (design and layout) will include street trees and other landscape materials, special paving, lighting, street furniture, public art and way-finding.

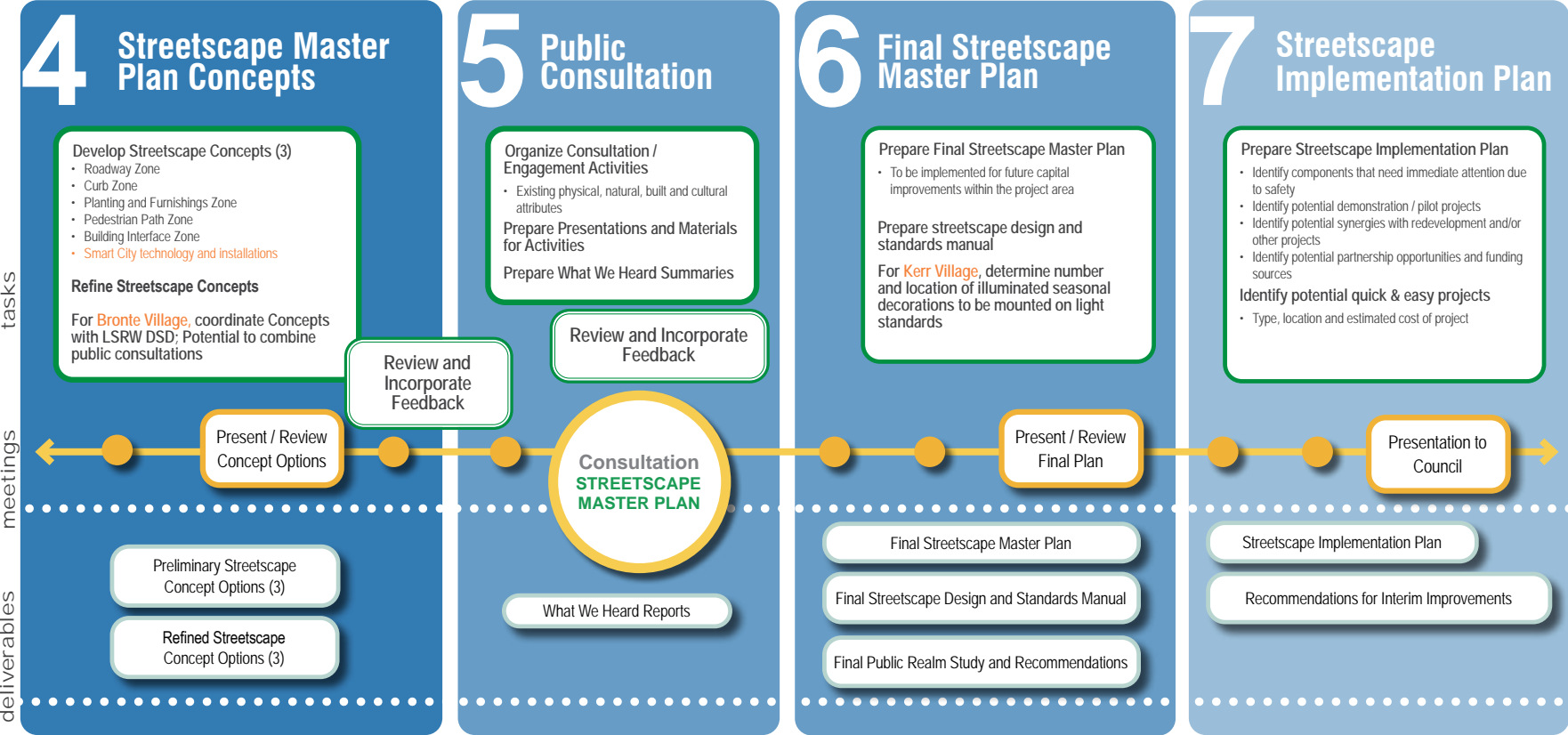


# Work Program



We Are Here

# Work Program





# What we've heard so far...

## One-on-one conversations with residents

40  
Conversations  
June 3, 2024

- **Opportunity and Appeal** - Described as a fantastic neighbourhood with a strong draw from restaurants and cafes. The Kerr Street Café and Maro's unique patios are key attractions.
- **Patios and Public Spaces** - Desire expressed for making it easier to have patios, using empty lots for temporary green spaces, and adding more seating and benches.

"Westwood Park is well done and well used"

"Love events like farmers markets (in Dorval crossing) it's a community event."

"Neighbourhood could use a lot more plants and trees, not a unified streetscape."

# What we've heard so far...

## One-on-one conversations with residents

- **Traffic and Parking** - Reduce parking to create more space for patios and walking destinations, traffic calming through narrower streets and greenery, and the possibility of a separated bicycle track.
- **Aesthetic and Infrastructure** - Emphasis on controlling patio designs, adding street lights with a unique identity, and creating more visually appealing and pedestrian-friendly streetscapes.

"Encourage people to use the GO by making the walk nicer"

"Need traffic calming – narrow street, greenery closer to the road, prioritizing for pedestrians – shopping gathering transit (not like Dorval which is a throughfare)"

"Too much cement, no focus on green infrastructure, risks flooding, more rain gardens and native plants; nothing else matters more than this. climate action."



# What we've heard so far...

## Conversation with the Kerr Village BIA

- Emphasis on bringing more people to the area by improving infrastructure and aesthetics, enhance Kerr Village's status as a tourist destination.
- Need for new light poles and fixtures.
- Desire for more uniform sidewalks and the transformation of Heritage Square back into green space from concrete.
- Praise for the town's horticulture department, with a need for water sources for hanging baskets and barrels.
- Suggestion to close Bond Street to increase park usage for events, leveraging its annual concert tradition.

# Kerr Village BIA

## BIA Action Plan Report 2015

J.C. Williams Group

- Consumer loyalty
- Area Improvements
  - Retailers
  - Events

### STRENGTHS

### OPPORTUNITIES

- Reasonable rents
  - Market feel
  - New retail
  - Festivals

- Walkability
- Retail mix
- Brand identity
- Customers
- Accessibility
- Buildings and Redevelopment

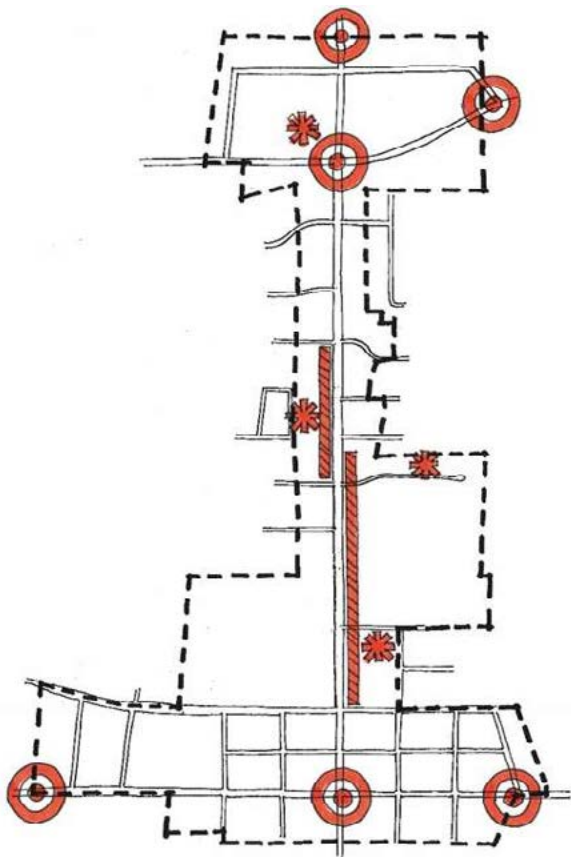
### WEAKNESSES

### THREATS

- Overall look - balance between beautification and high prices
- Consumers prefer simplicity

# Direction from the Town

## Urban Design Direction for Kerr Village Growth Area



### distinguish the district

- create a sense of arrival
- incorporate unifying elements
- incorporate expressive elements

*'distinguishing the district'* design direction is primarily focused on:



urban squares



gateways

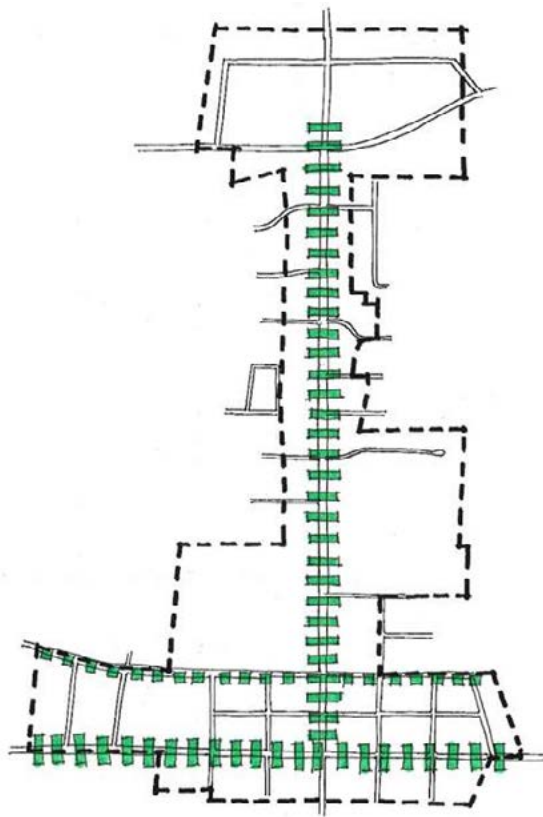


enhanced streetscape setbacks



# Direction from the Town

## Urban Design Direction for Kerr Village Growth Area



### foster activity at street level

- create active spaces on the boulevard
- incorporate active uses on the ground floor

### *'fostering activity at the street level'*

design direction is primarily focused on:



primary streets



secondary streets



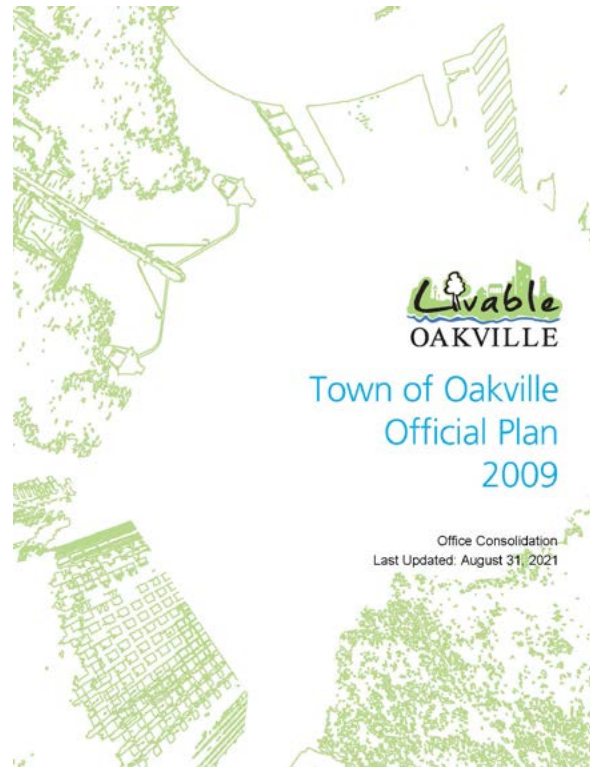
*Kerr Village at build-out under current policies*

# Direction from the Town

## Livable Oakville Official Plan 2009 (OP Review 2017)

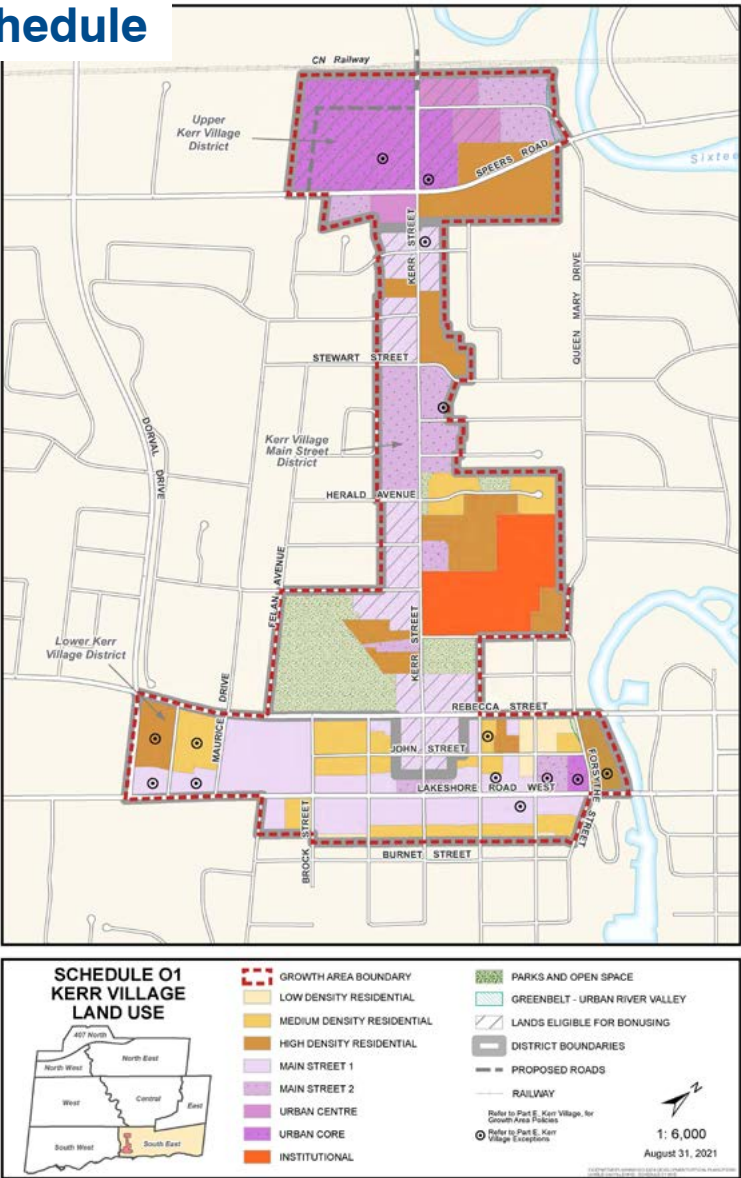
- Promote **compact urban form** with higher density development through compatible development and redevelopment opportunities
- Enhance mobility for all through transit priority measures and increase levels of service through the development process by promoting **pedestrian and cycling-oriented mixed use development, improving pedestrian and cycling circulation and connections**, and increasing efficiencies for alternate modes of transportation
- Create an **attractive public realm** by promoting high quality streetscapes and open spaces, and **ensuring appropriate transitions** between main street areas and lower density residential neighbourhoods.

*Part E: Growth Areas, Special Policy Areas and Exceptions*  
*Section 23.2 Objectives*



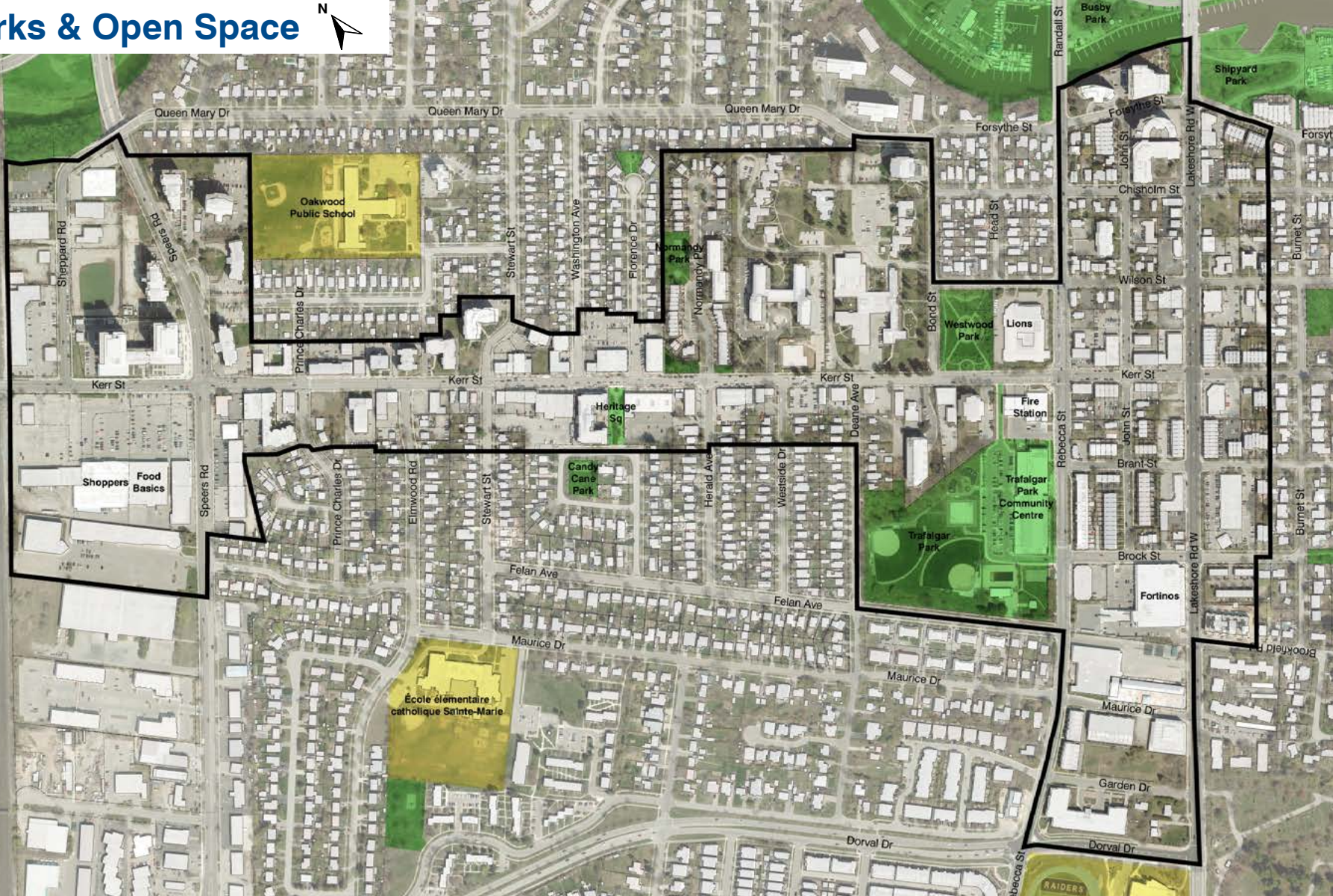


# Kerr Village Land Use Schedule



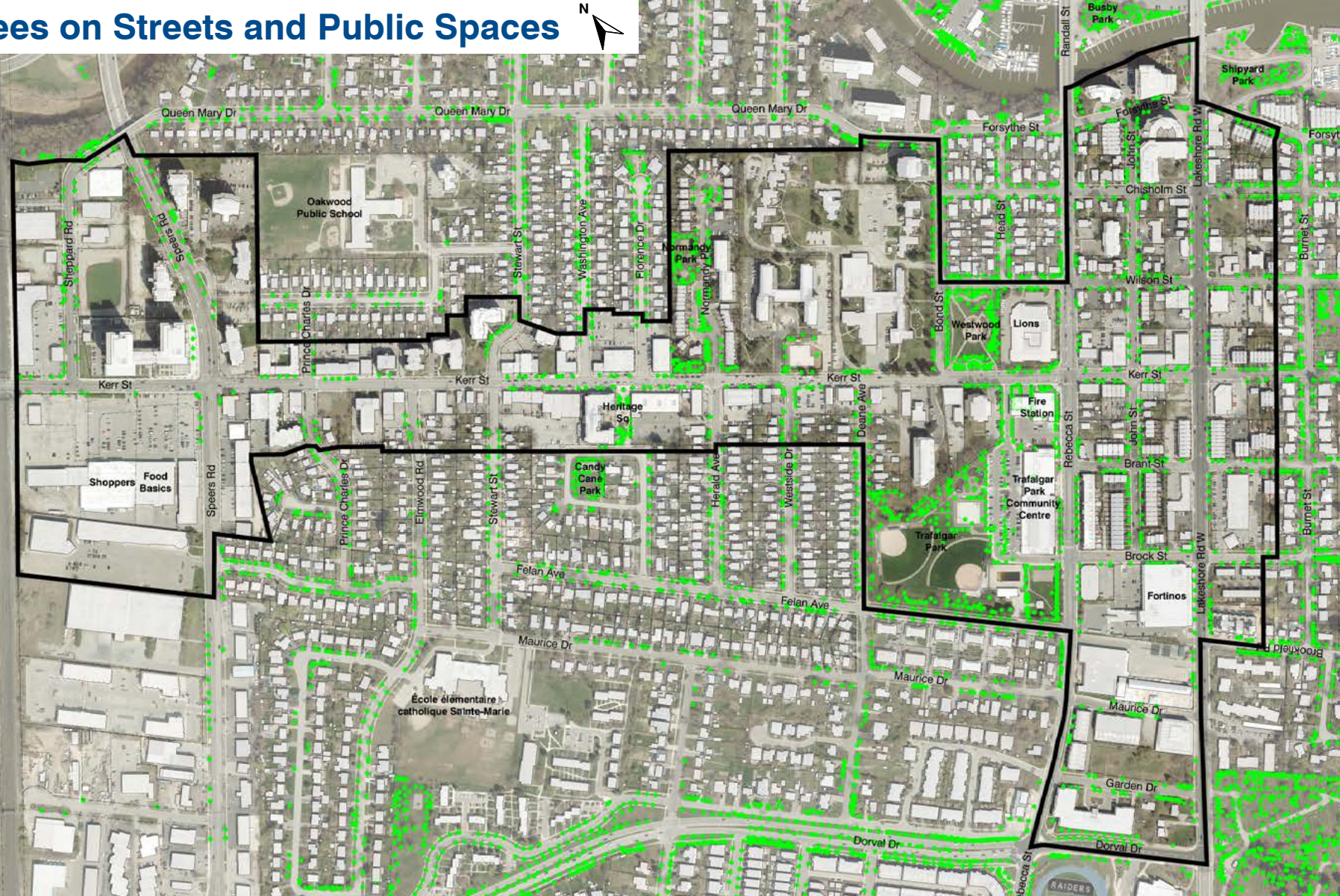


# Parks & Open Space

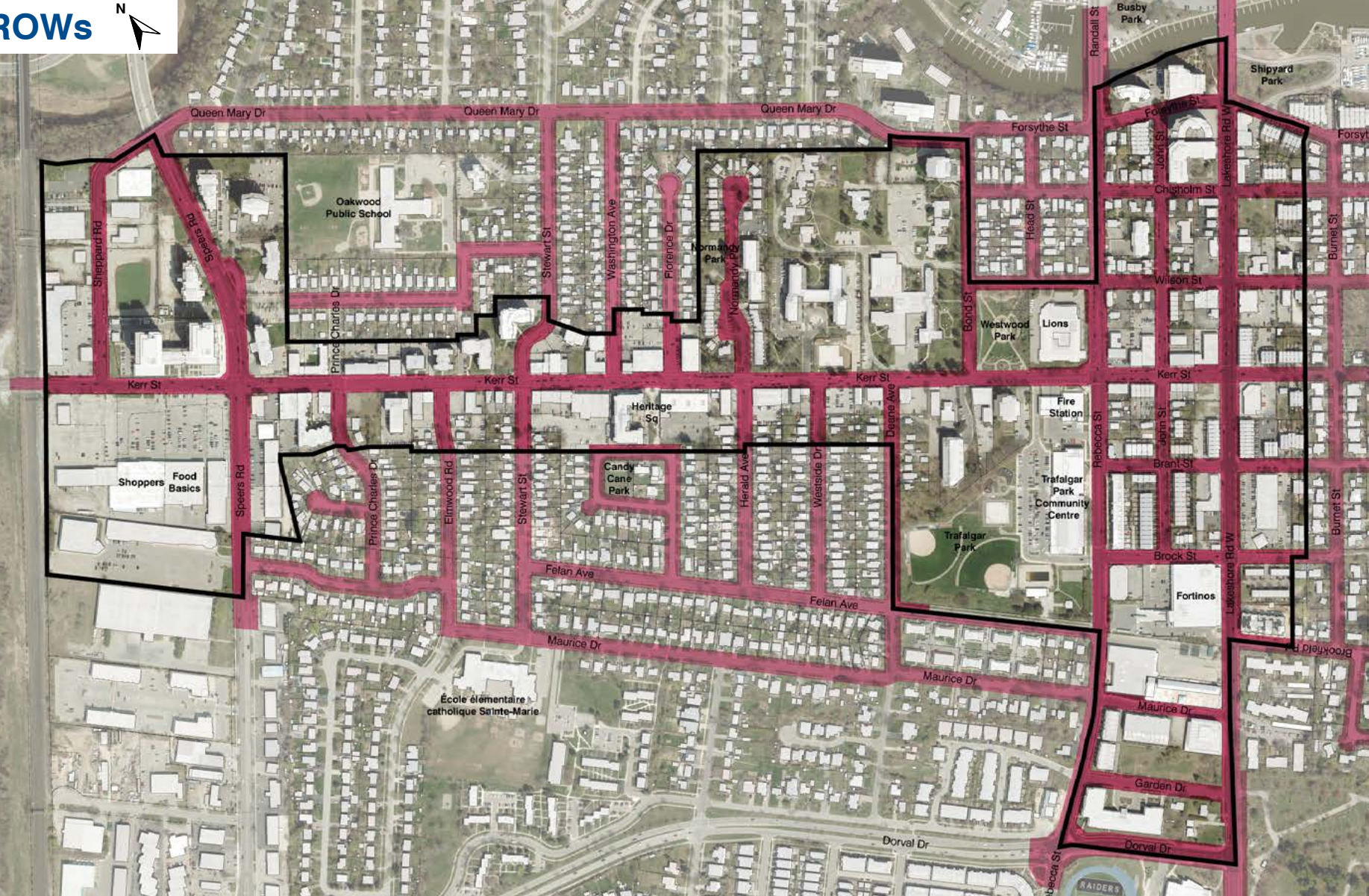




# Trees on Streets and Public Spaces



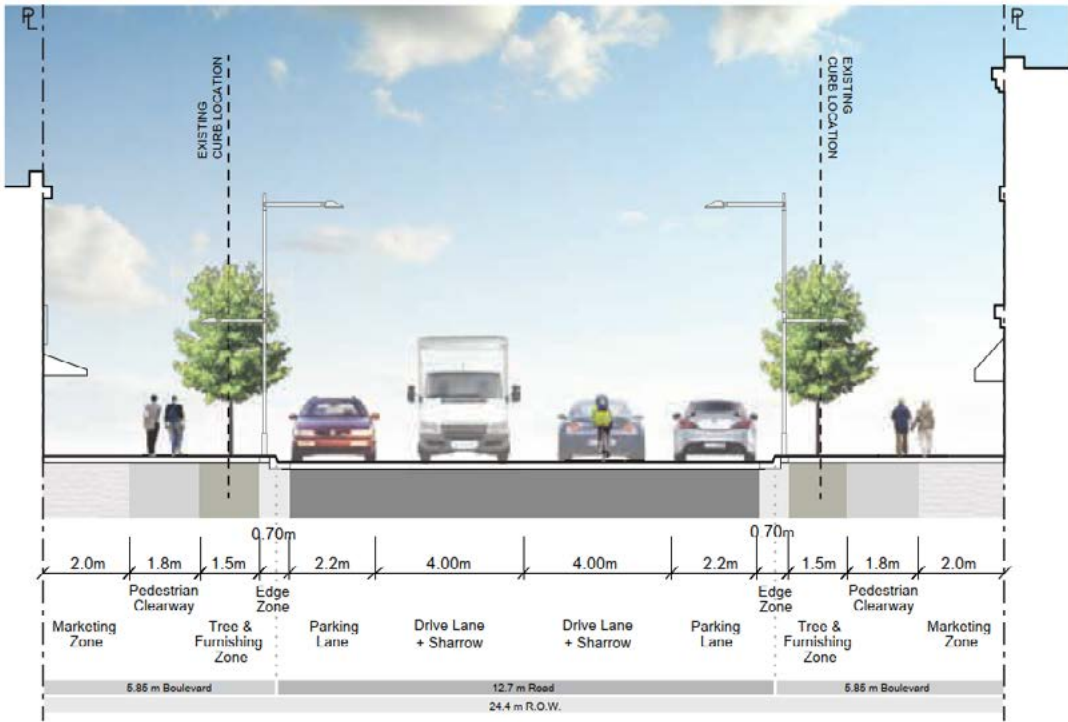






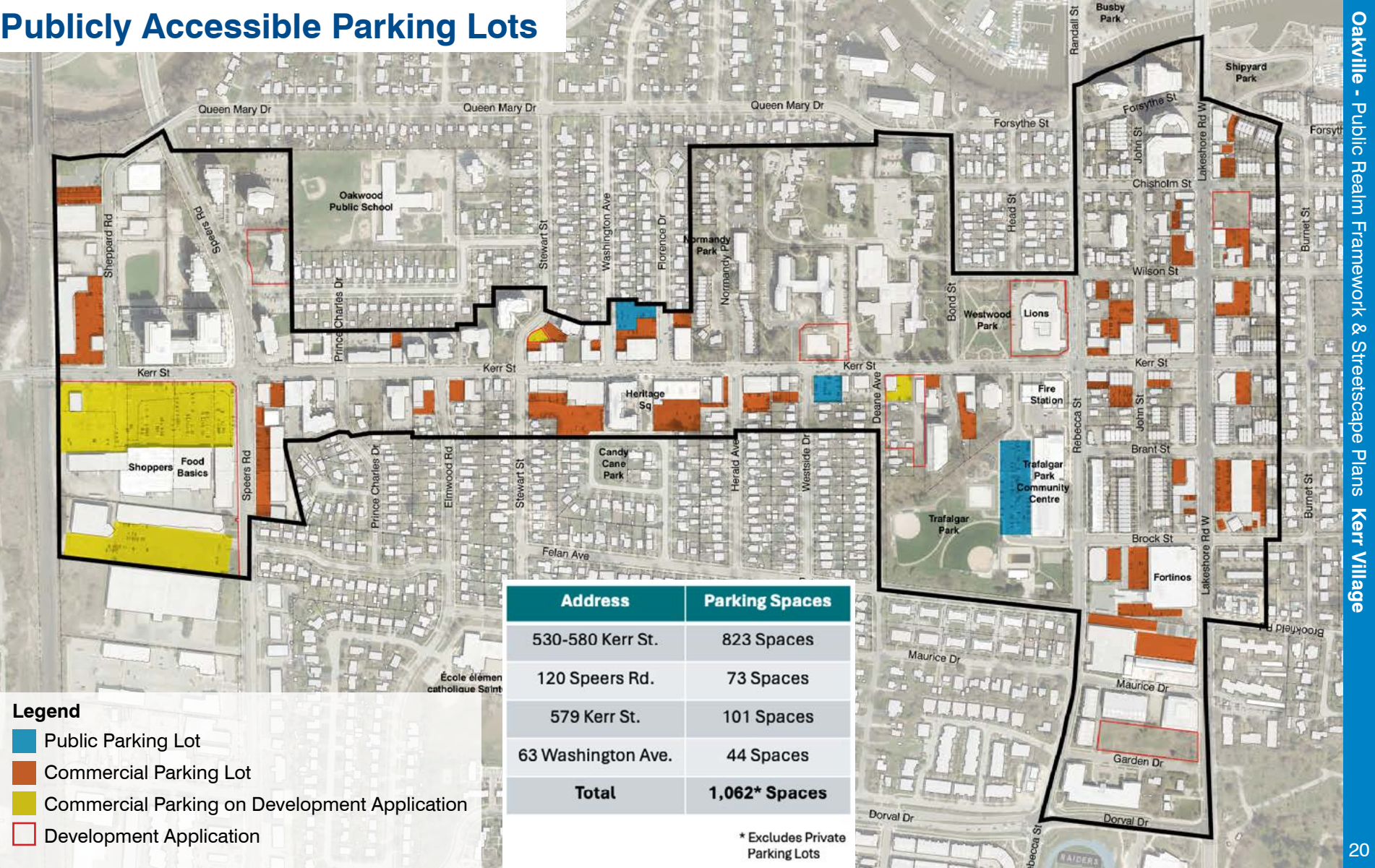
# Downtown Streetscape Master Plan: Lakeshore Road

- Proposed Cross-Section (24.4 m ROW)



Cross-Section Element	Width
Marketing Zone	2.0 m
Pedestrian Clearway	1.8 m
Tree & Furnishing Zone	1.5 m
Edge Zone	0.70 m
Parking Lane	2.2 m
Drive Lane + Sharrow	4.0 m
Drive Lane + Sharrow	4.0 m
Parking Lane	2.2 m
Edge Zone	0.7 m
Tree & Furnishing Zone	1.5 m
Pedestrian Clearway	1.8 m
Marketing Zone	2.0 m

# Publicly Accessible Parking Lots





# On-Street Parking



Street	On-Street Parking
Kerr St.	127 Spaces
Wilson St.	11 Spaces
Christholm St.	15 Spaces
Maurice Dr.	56 Spaces
Lakeshore Rd W.	42 Spaces
John St.	33 Spaces
Deanne Ave.	31 Spaces
Prince Charles Dr.	5 Spaces
Washington Ave.	3 Spaces
Florence Dr.	2 Spaces
Bond St.	9 Spaces
Total	334 Spaces

**Legend**

■ On-Street Parking

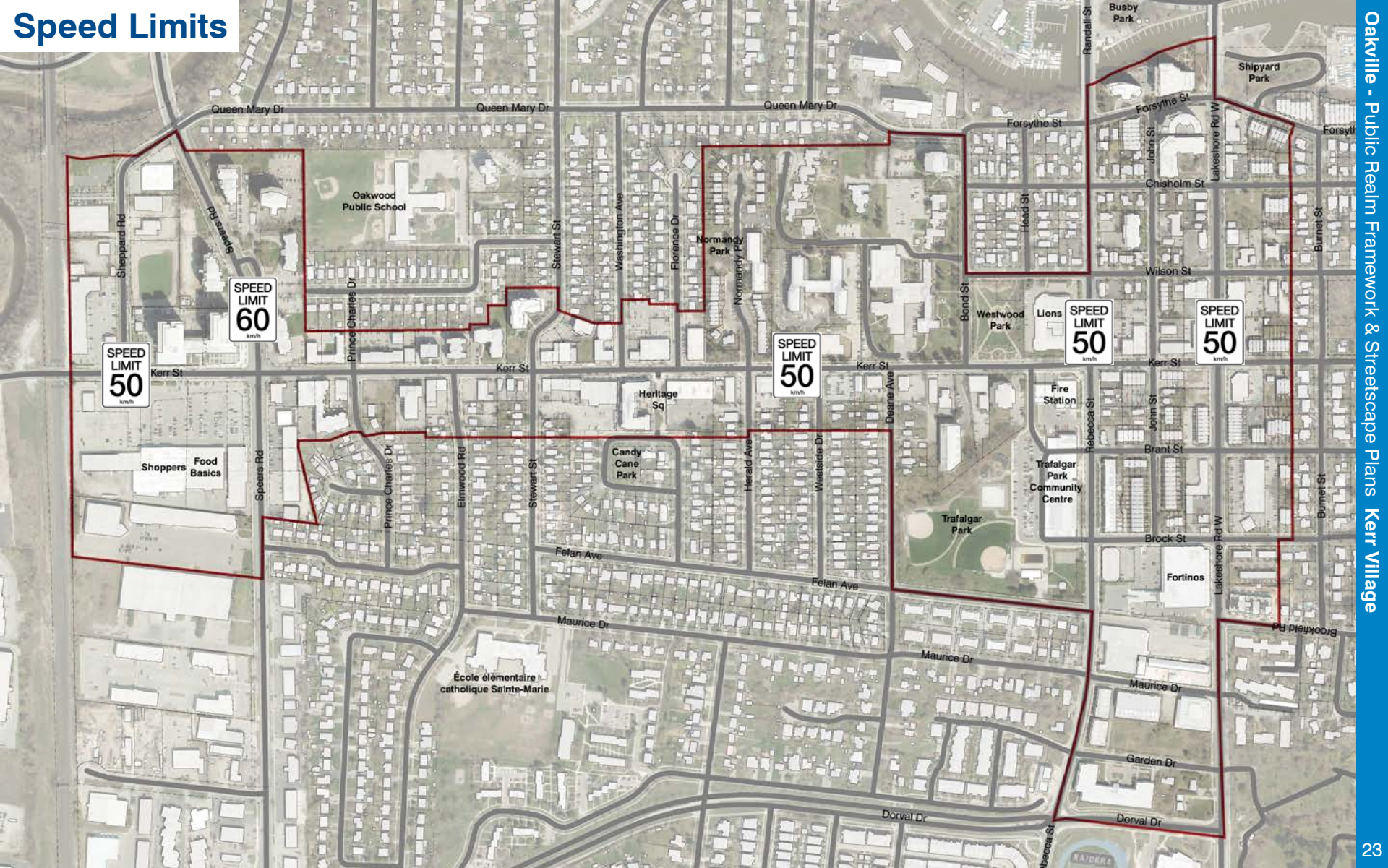


## Oakville - Public Realm Framework & Streetscape Plans Kerr Village



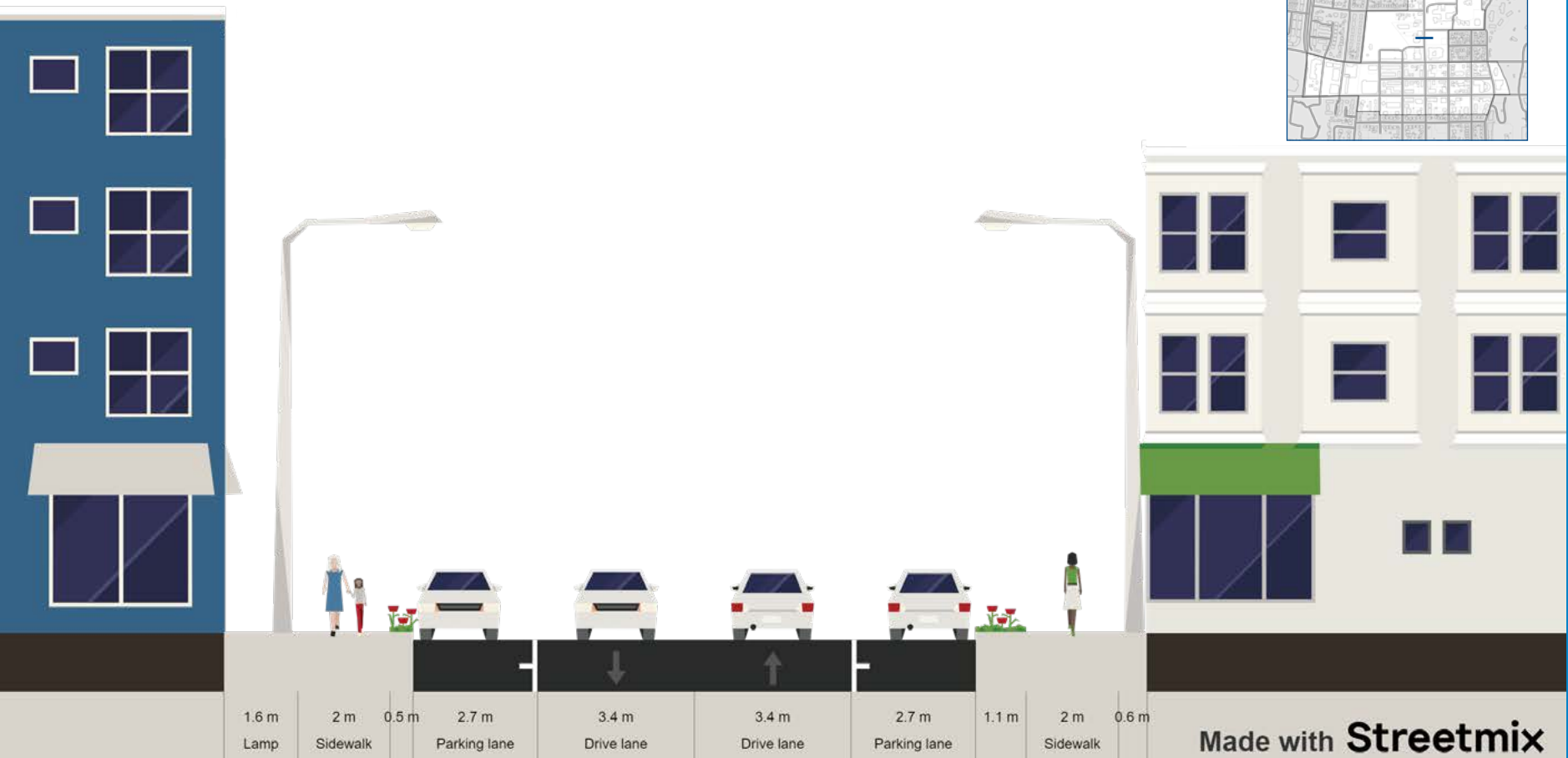


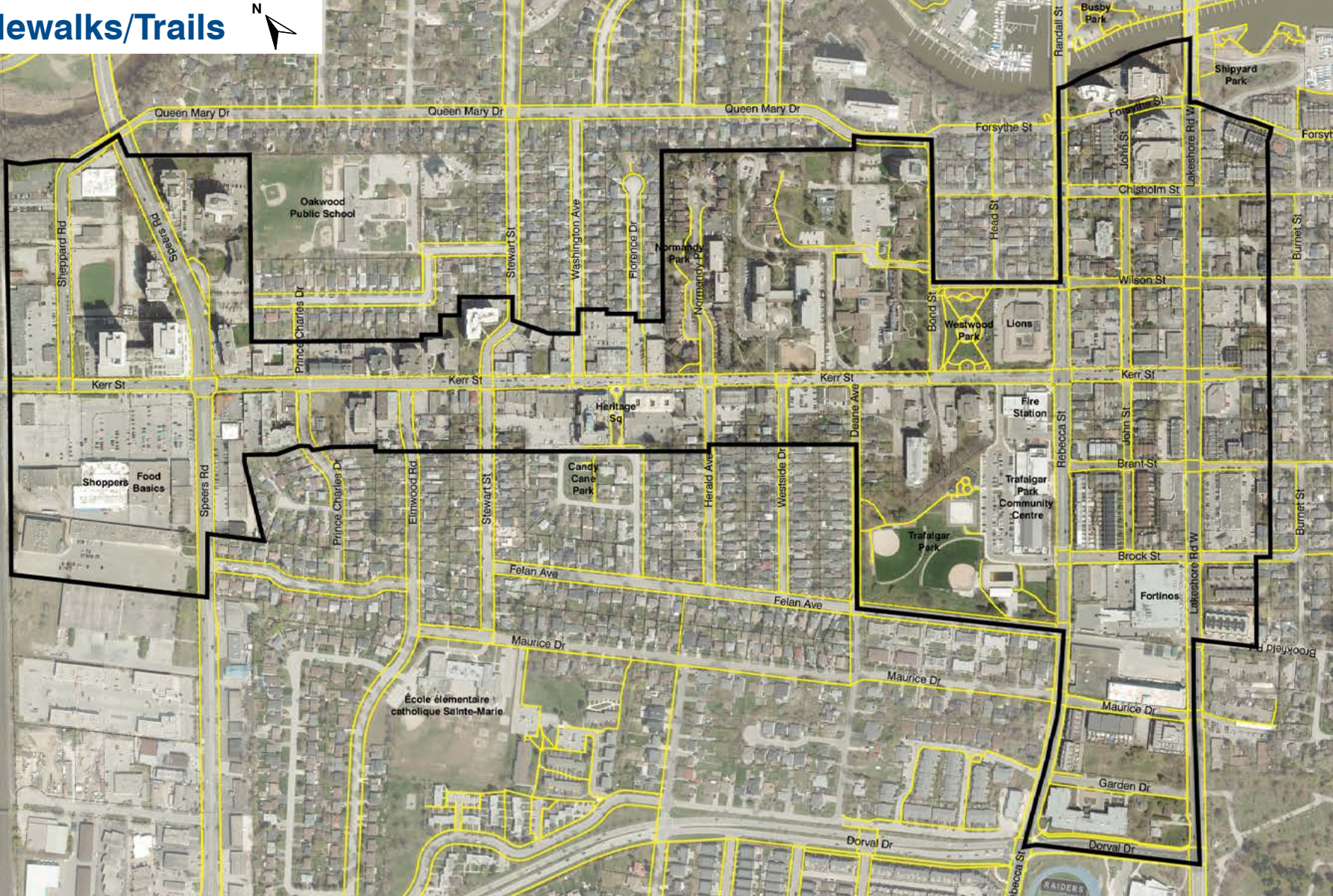
# Speed Limits





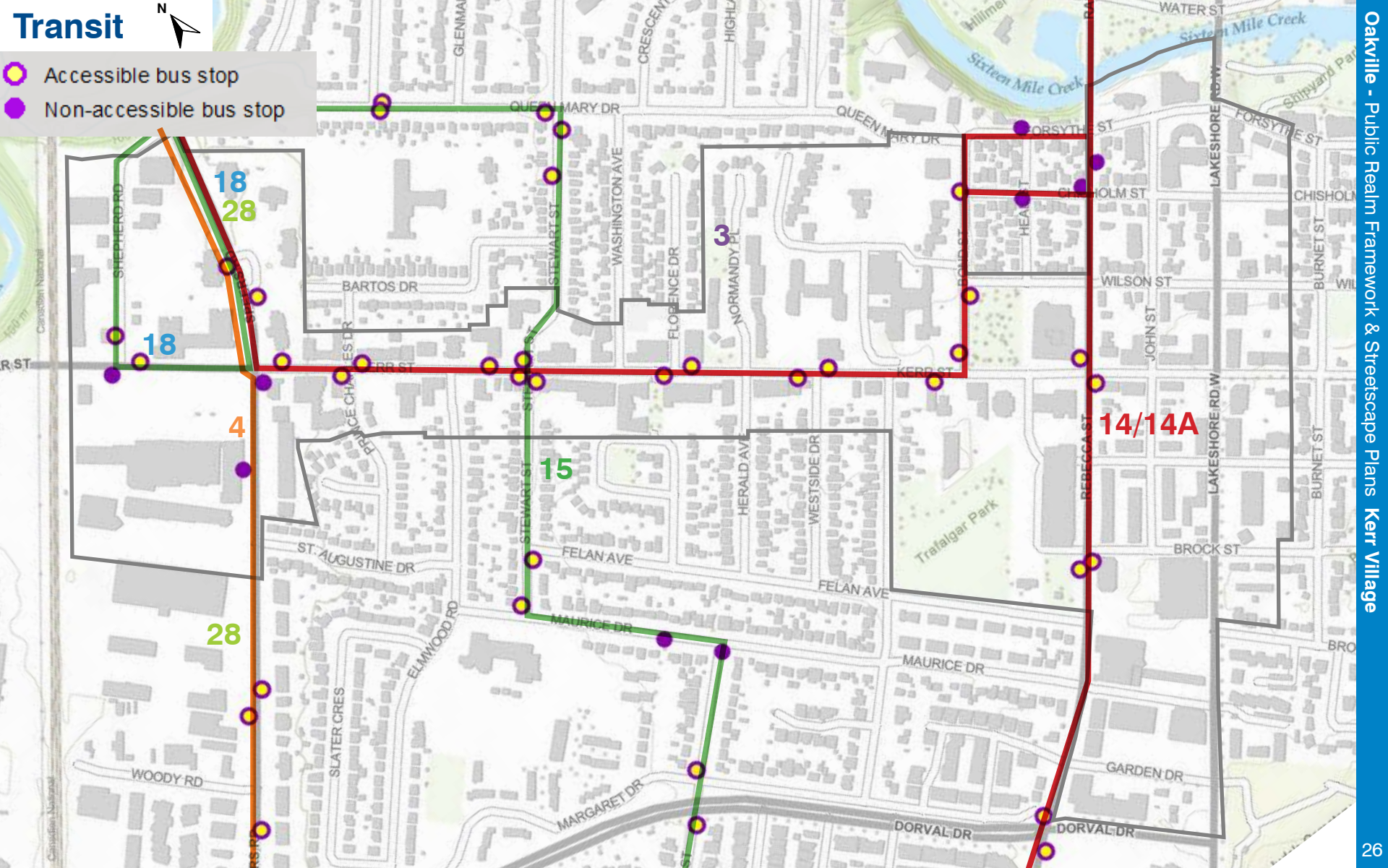
# Existing Cross-Section: Kerr Street (20 m ROW)



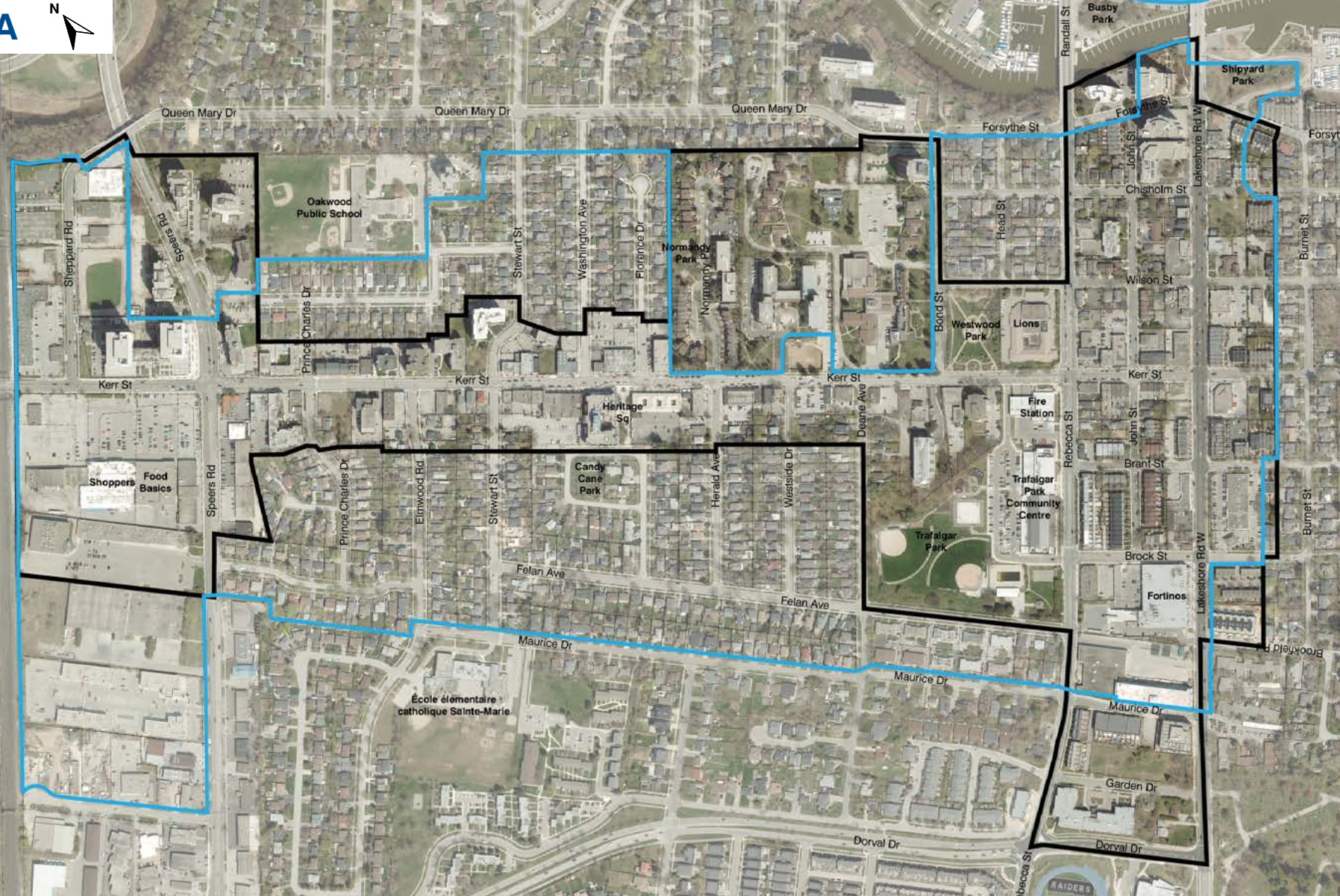




- Accessible bus stop
- Non-accessible bus stop

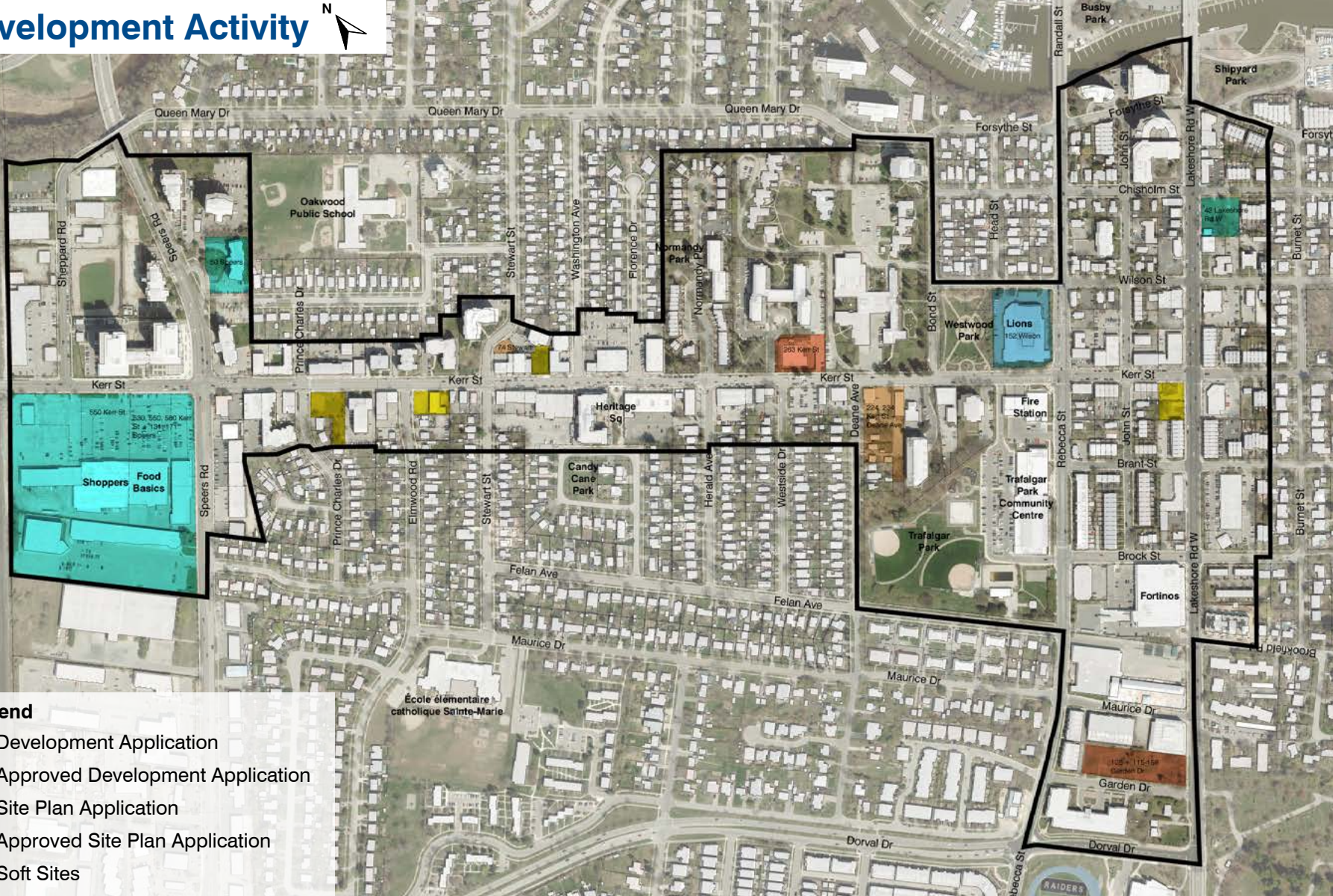








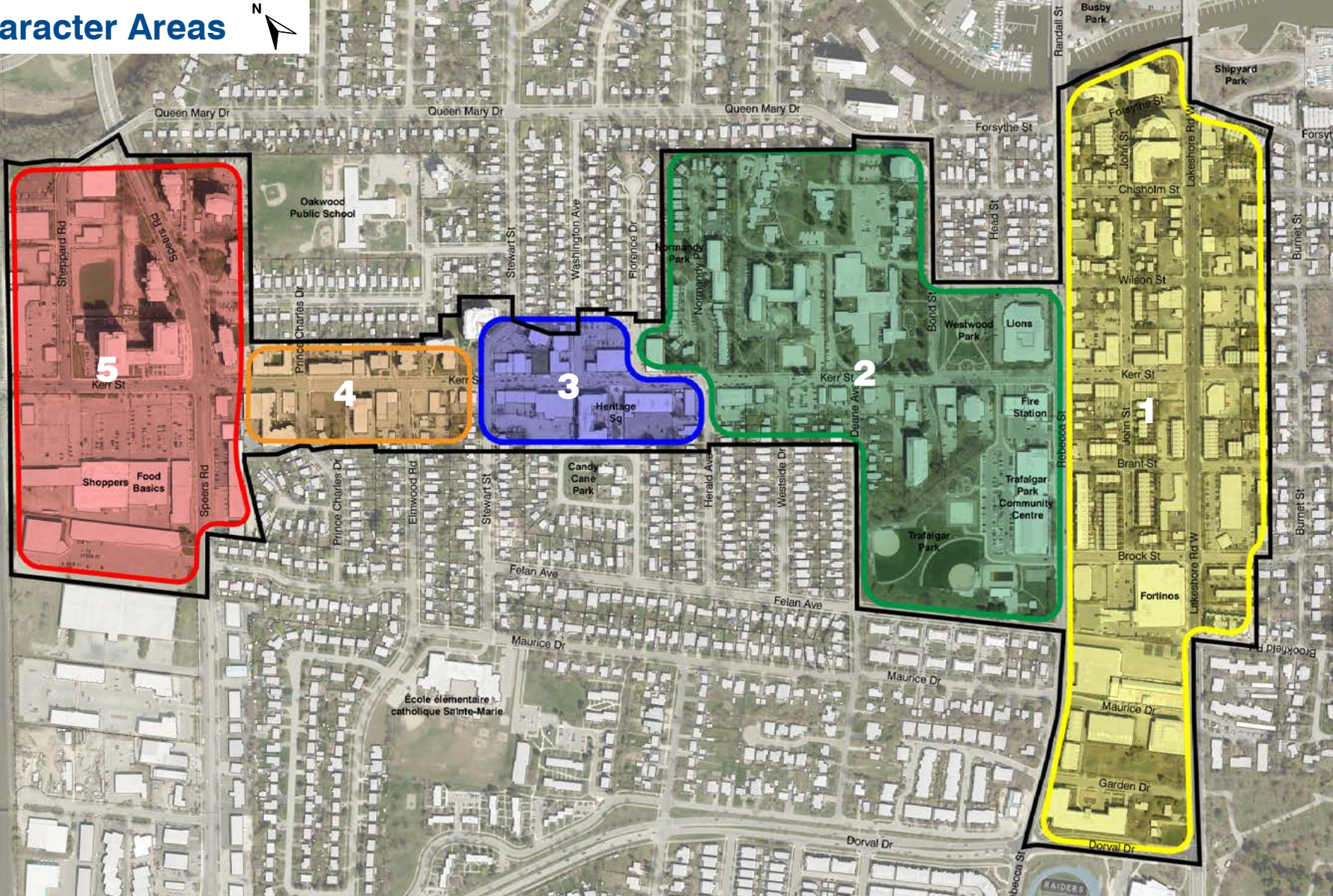
# Development Activity



- Legend**
- Development Application
  - Approved Development Application
  - Site Plan Application
  - Approved Site Plan Application
  - Soft Sites



# Character Areas





# Character Areas

## Zone 1 - South of Rebecca St.

- Main Street shopping area with retail edges including food and beverage.
- Terraces along west edge create accessibility issues leading into retail suites.
- Parking bays and bump outs at intersections on Kerr St.
- Streetscape includes trees in small planting beds, seasonal containers and concrete planters.
- Two large redevelopment sites at the NW and SW corners of Lakeshore Rd.

### Opportunities

- Re-imagine greening of the street and the potential removal of the large concrete planters that consume a lot of space in a narrow corridor.
- Re-imagine lighting approach.
- Explore opportunities to link Kerr Village to the lakefront park
- Create a stronger gateway to the village at Lakeshore Rd.
- Anticipate the development / redevelopment of the corner lots of Lakeshore Rd and Kerr St.





# Character Areas

## Zone 2 - Rebecca St. to Herald Ave.

- Park setting with mix of uses including large scale residential and institutional on the east side characterized by lawns and treed frontages and one sided low rise retail on the west side.
- Parking bays and bump outs line the edges of the street.

### Opportunities

- Potential to provide public realm amenities, along the street edges of the institutional properties adjacent to the corridor.
- Strengthen connections to Westwood Park and to Trafalgar Park from Kerr Street.
- Re-imagine lighting approach.



# Character Areas

## Zone 3 - Herald Ave. to Stewart St.

- Core area retail
- Low scale, 1-2 storey retail including cafes and food service.
- Heritage Square was a central gathering space.
- Intersection bump outs and parking bays.

### Opportunities

- Strengthen legibility of Village Core and consider pedestrian amenities.
- Remove large planters, re-imagine greening strategy and re-design Heritage Square.
- Re-imagine lighting approach.





# Character Areas

## Zone 4 - Stewart St. to Speers Rd.

- Mixed Use corridor dominated by larger scale residential uses with some low scale pocket retail.
- Typical frontages include lawns, parking areas and some at grade retail.
- More suburban in character.
- No bump outs or parking bays.

### Opportunities

- Consider bump outs and parking bays similar to streetscape approach south of Stewart Street.
- Re imagine lighting approach.
- Consider greening approach for the street.



# Character Areas

## Zone 5 - Upper Kerr

- Large format retail and large scale mixed use development characterized by large open parking lots.
- Major redevelopment of this area is anticipated into a mixed-use community.

### Opportunities

- Integrate the future development scenario as the north terminus of Kerr Village.
- Consider ideal locations for new public spaces.
- Safe crossing of Speers Road.

















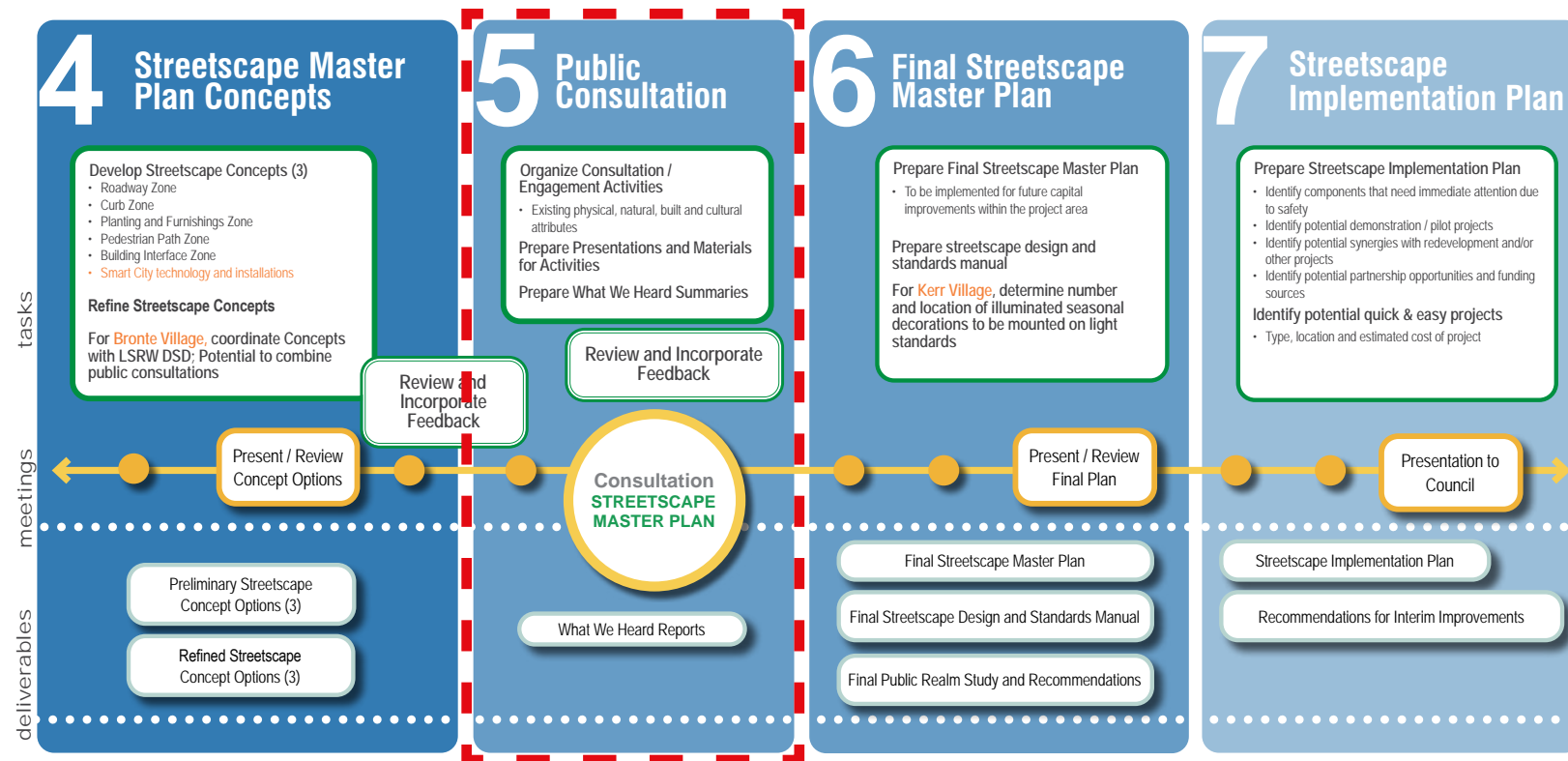






# Next Steps

Contact: [christina.tizzard@oakville.ca](mailto:christina.tizzard@oakville.ca)





# Workshop Activity

Each table has an air photo map of the Kerr Street area and Heritage Square. Write notes on the side or on the map. Share your thoughts on:

## **Parks, Squares and Open Space**

Think about how to improve the spaces. Think about places you've visited and describe what makes them memorable - what could be considered for Kerr?

## **Streetscape**

Think about trees and other landscape, sidewalk width, furnishings, public art

## **Moving Around**

Think about traffic, pedestrian routes and crossing, parking on the street and in lots, cycling lanes, signage

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