



Technical Memorandum

To:	Syed Rizvi – Town of Oakville	Date:	2019-11-29
Cc:	Kevin Singh – ARGO Development Corporation Christopher Gordon – CGH Transportation		
From:	Mark Crockford, P.Eng.	Project Number:	2019-78

Re: Argo West Morrison Creek – Transportation Impact Study Context

Introduction / Context

Through the pre-consultation meeting for ARGO's West Morrison Creek development a scoped transportation impact study (TIS) has been requested to support the draft plan application. A Terms of Reference for the scoped TIS was drafted and discussed with Syed Rizvi at the Town of Oakville. The scoping email has been included as Attachment 1.

This memo has been prepared within the context of a larger Transportation Impact Study that was prepared for several properties in the North Oakville Neighbourhoods 9/10/11 Area. That TIS, the Neighbourhood 9/10/11 TIS, covered the TIS requirements, per the Halton Region TIS Guidelines and the North Oakville Terms of Reference for Transportation Impact Studies.

This memo has been prepared to address some site-specific considerations for Argo's West Morrison Creek including Transit Facilities Plan, Road Cross-Sections, Pedestrian Circulation Plan, Cycling Facilities Plan, and Parking Provisions.

Transit Facilities Plan

A Transit Facilities Plan was created as part of the Neighbourhood 9/10/11 TIS. The Transit Facilities Plan has been recreated, focusing on the West Morrison Creek Development. This plan is included in Drawing 001, Attachment 2. Per the Transit Facilities Plan, all of the residential units are within 400 metres of at least one proposed transit station.

Road Cross-sections

The proposed right-of-ways and cross-sections are illustrated on Drawing 002, Attachment 2. These cross-sections are proposed to be consistent with the North Oakville Urban Design and Open Space Guidelines. The cross-sections used in this plan have been included in Attachment 3. There is one exception, the proposed 11.0 metre cross-section for the laneway between townhouses. A proposed cross-section for the 11.0 metre cross-section is included in Attachment 3.

Pedestrian Concept Plan

A pedestrian circulation plan has been created to illustrate the sidewalk locations. Sidewalk provisions are generally per the road cross-section. Drawing 003, Attachment 2, illustrates the sidewalk locations and key pedestrian crossing locations. Additionally, a Pedestrian Circulation Plan has been prepared by Korsiak Planning and is included in Attachment 4.

Cycling Facilities Plan

A Cycling Facilities Plan was created as part of the Neighbourhood 9/10/11 TIS. The Cycling Facilities Plan has been recreated, focusing on the West Morrison Creek Development. This plan is included in Drawing 004, Attachment 2. Per the Cycling Facilities Plan, a signed route is provided along Marvin Avenue and a bicycle lane is provided along Sixth Line.

Parking Provisions

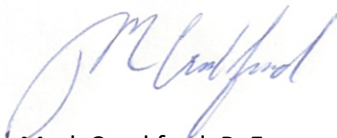
Parking provisions for the proposed West Morrison Creek development have been prepared by Korsiak Planning. A Parking Provisions Plan is included in Attachment 4.

Conclusions

The proposed development will have adequate cycling, pedestrian, and transit facilities. The proposed right of ways are sufficient to support the appropriate cross-sections. The Neighbourhood 9/10/11 TIS included all operational analysis to support the proposed development and no further analysis is required.

If you have any questions or comments, please do not hesitate to contact the undersigned.

Prepared By:



Mark Crockford, P. Eng.

P: 905-251-4070

Mark.Crockford@CGHTransportation.com

Reviewed By:



Christopher Gordon, P. Eng.

P: 343-999-9117

Christopher.Gordon@CGHTransportation.com

Attachment 1

Scoping Email Correspondence

Mark Crockford

From: Mark Crockford
Sent: November 21, 2019 1:44 PM
To: Syed Rizvi
Cc: Christopher Gordon; Kevin Singh
Subject: Argo West Morrison Creek

Hi Syed,

Thanks for taking the time to call me again, I appreciate being able to discuss this with you.

As per our phone conversation the Neighbourhood 9/10/11 TIS was prepared to support several developments within the area, including Argo's West Morrison Creek development. This previous study covers the required traffic analysis to support Argo's development application as it includes both of the access intersections to the proposed development (Marvin Avenue/ Access #6 at Sixth Line and Carnegie Drive/Access #7 at Sixth Line). To cover the additional transportation related items a covering letter will be prepared that addresses the following:

- Pedestrian Circulation Plan
- Transit Facilities Plan
- Road Cross-sections
- Parking Provisions

No additional Synchro analysis or traffic volume projections are anticipated as part of the covering letter.

Thanks again,

Mark



Mark Crockford, P.Eng.
CGH Transportation Inc.
P:905-251-4070
E:Mark.Crockford@CGHTransportation.com

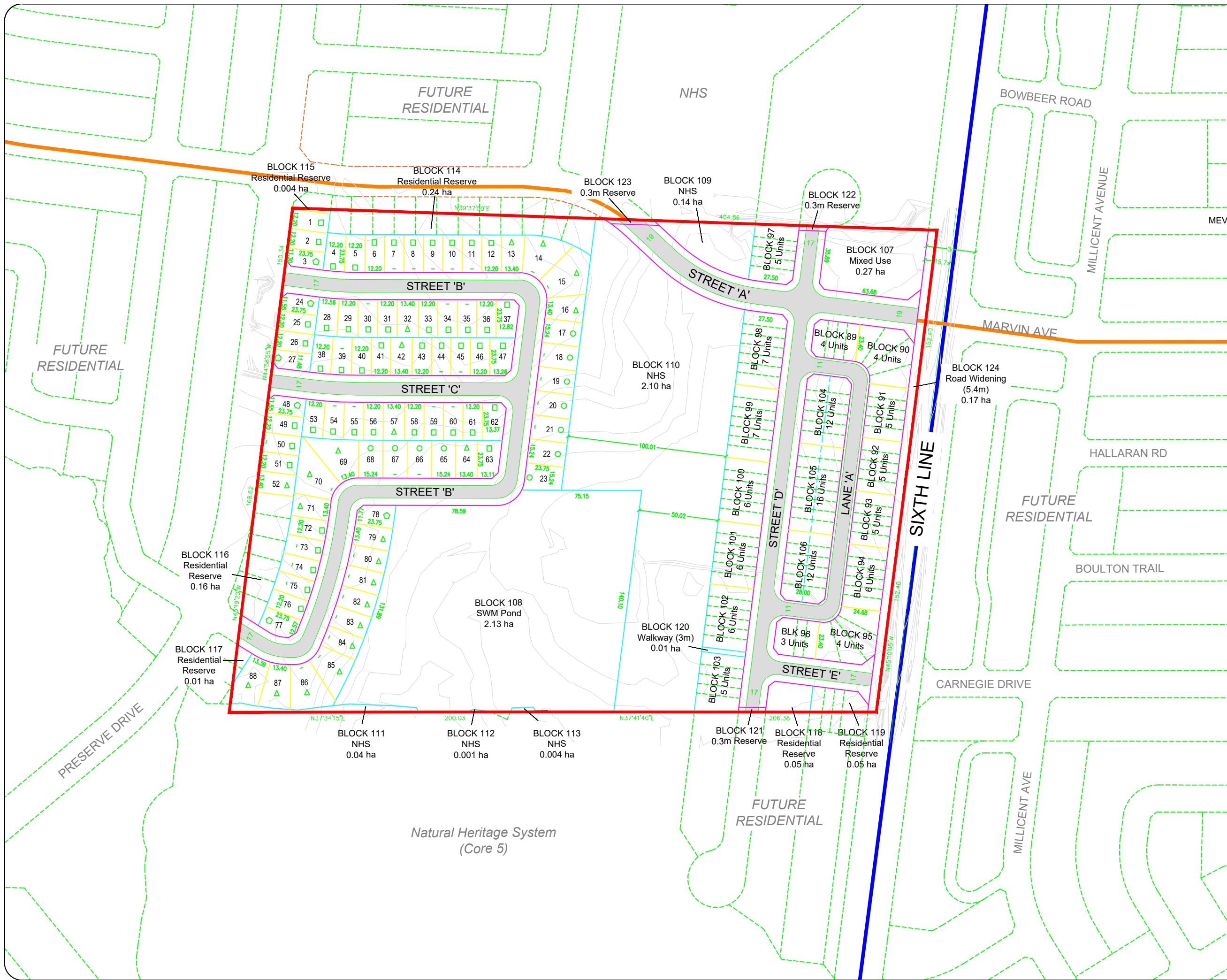
Attachment 2

Transit Facilities Plan

Road Cross-Section Plan

Pedestrian Concept Plan

Cycling Facilities Plan



Notes:

LEGEND:

- Signed Route
- Bicycle Lane
- Multi-Use Pathway

A	description	by	xx/xx/xx
REV:	DESCRIPTION:	BY:	DATE:
STATUS:		status	



CGH Transportation
 628 Haines Road
 Newmarket, ON
 L3Y 6V5
 (905) 251-4070

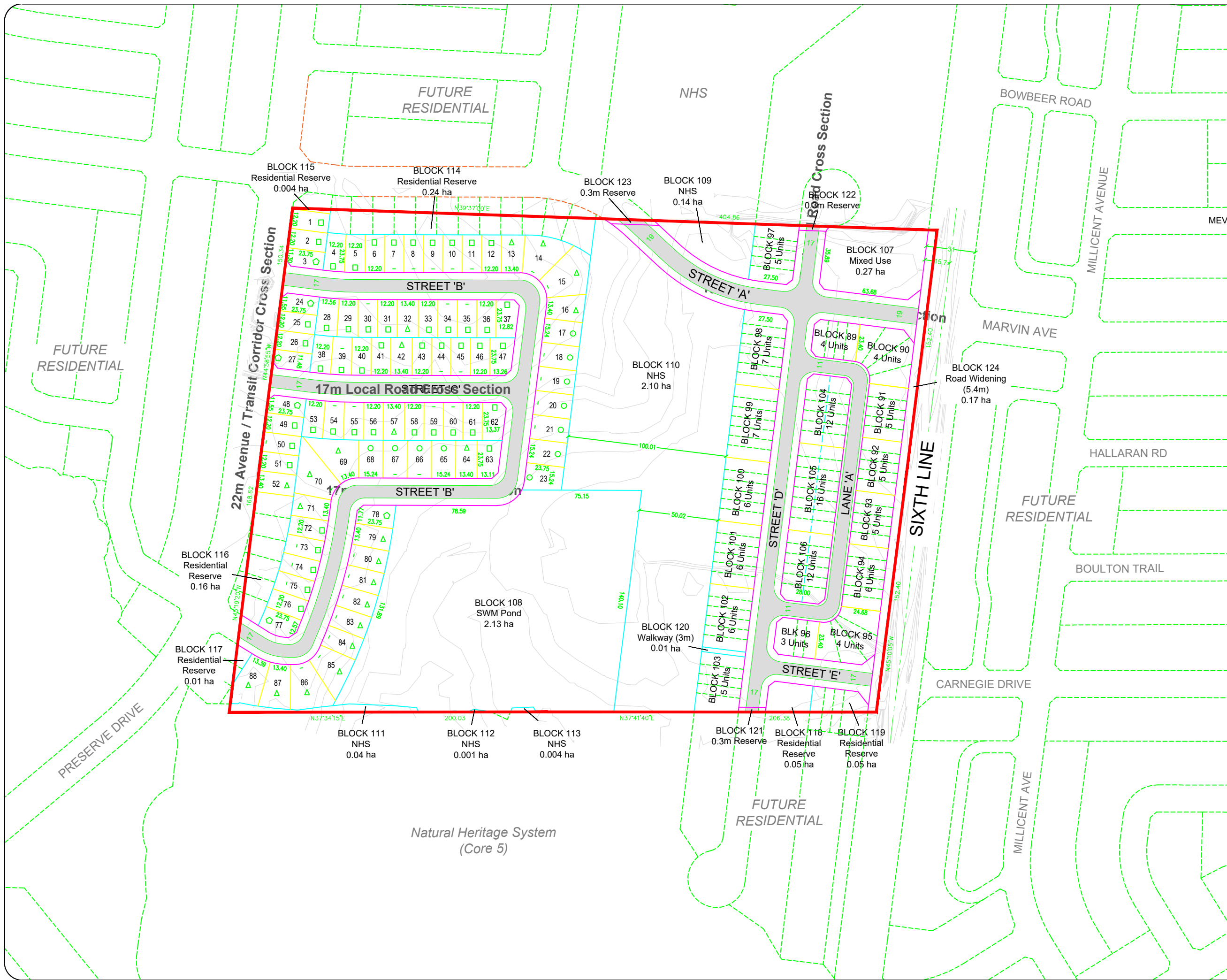
CLIENT: Argo Development Corporation
 Argo West Morrison Creek

ARCHITECT:

SITE: Neighbourhood 9/10

TITLE: Cycling Facilities Concept Plan

SCALE AT A3: NTS	DATE: 2019-12-18	DRAWN: MC	CHECKED:
PROJECT NO: 2019-78	DRAWING NO: 004	REVISION:	



Notes:

A	description	by	xx/xx/xx
REV:	DESCRIPTION:	BY:	DATE:
STATUS:		status	



CGH Transportation
 628 Haines Road
 Newmarket, ON
 L3Y 6V5
 (905) 251-4070

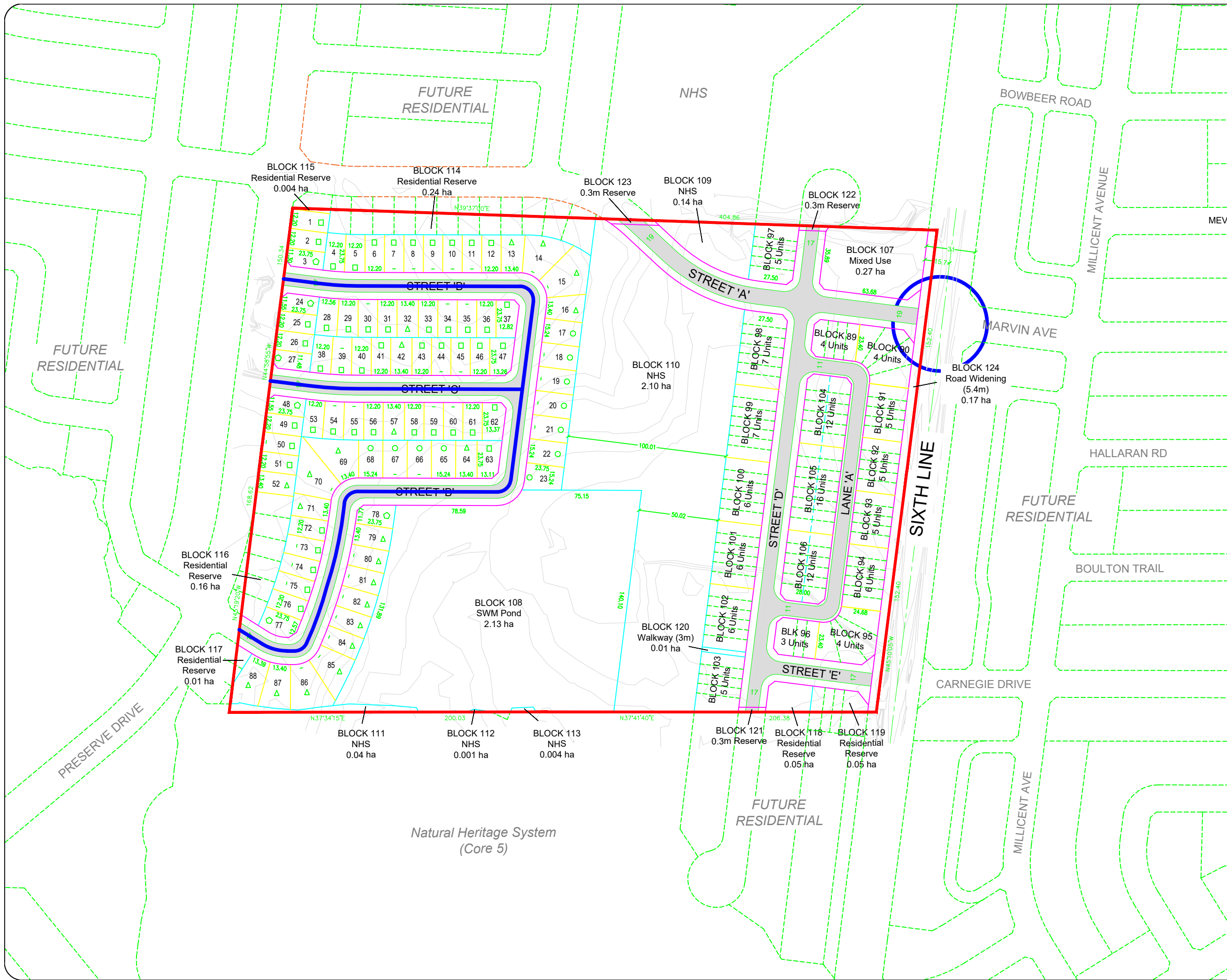
CLIENT: Argo Development Corporation
 Argo West Morrison Creek

ARCHITECT:

SITE: Neighbourhood 9/10

TITLE: Road Cross-Section Concept Plan

SCALE AT A3:	DATE:	DRAWN:	CHECKED:
NTS	2019-12-18	MC	
PROJECT NO:	DRAWING NO:	REVISION:	
2019-78	002		



Notes:

LEGEND:

- Single Side Sidewalk
- Both Side Sidewalk
- Pedestrian Crossing

A	description	by	xx/xx/xx
REV:	DESCRIPTION:	BY:	DATE:
STATUS:		status	

CGH Transportation
 628 Haines Road
 Newmarket, ON
 L3Y 6V5
 (905) 251-4070

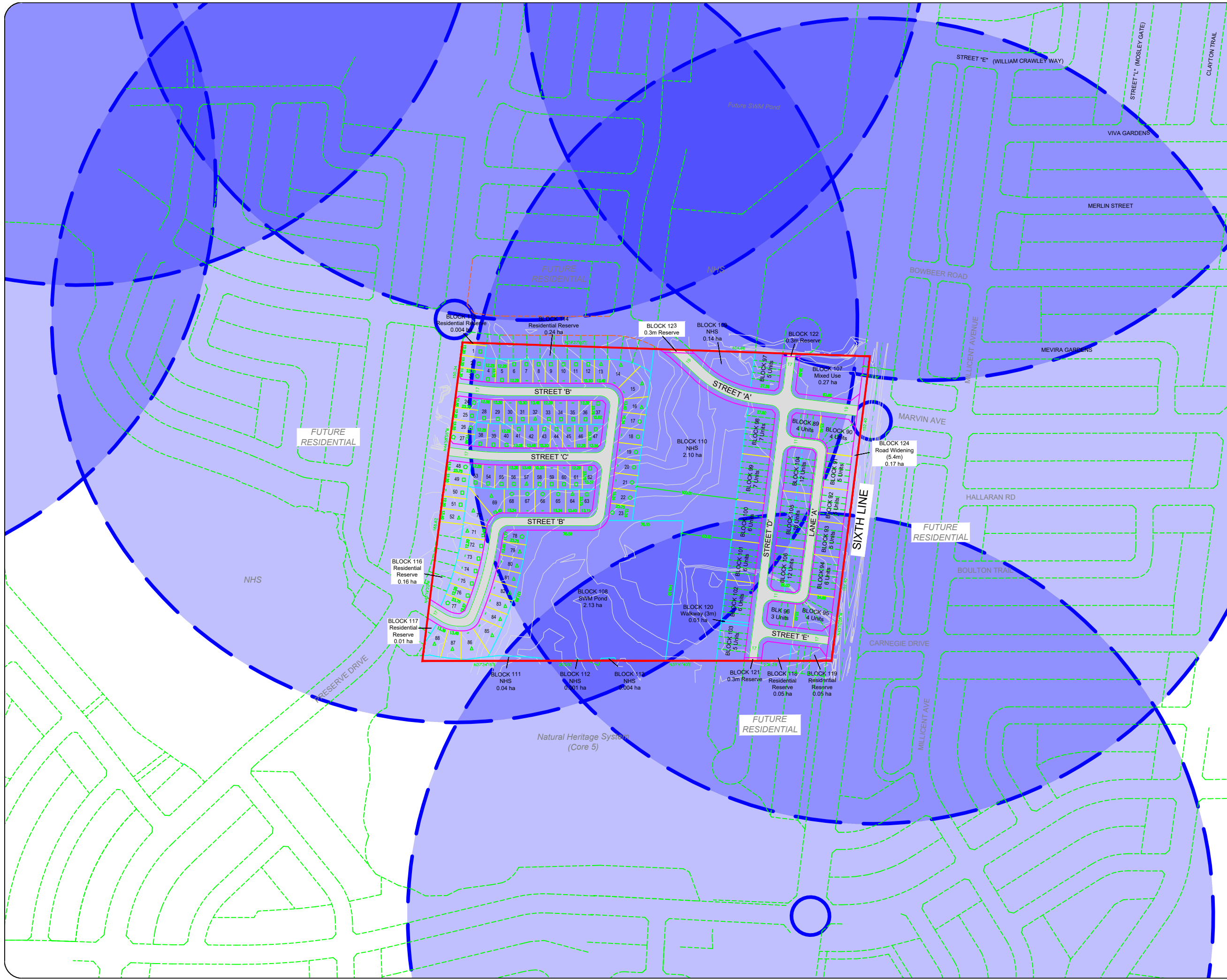
CLIENT: Argo Development Corporation
 Argo West Morrison Creek

ARCHITECT:

SITE: Neighbourhood 9/10

TITLE: Cycling Facilities
 Concept Plan

SCALE AT A3: NTS	DATE: 2019-12-18	DRAWN: MC	CHECKED:
PROJECT NO: 2019-78	DRAWING NO: 003	REVISION:	



Notes:

LEGEND:
 400m Transit Walking Distance

A	description	by	xx/xx/xx
REV:	DESCRIPTION:	BY:	DATE:
STATUS:		status	



CGH Transportation
 628 Haines Road
 Newmarket, ON
 L3Y 6V5
 (905) 251-4070

CLIENT: Argo Development Corporation
 West Morrison Creek

ARCHITECT:

SITE: Neighbourhood 9/10

TITLE: Transit Facilities
 Concept Plan

SCALE AT A3: NTS	DATE: 2019-12-18	DRAWN: MC	CHECKED:
PROJECT NO: 2019-78	DRAWING NO: 001	REVISION:	

Attachment 3

Proposed Cross-Sections

5.2.2. Avenue/Transit Corridor

Between the Arterial/Transit Corridors, the Avenue/Transit Corridors provide a finer grid of connection within and between neighbourhoods. These connections provide alternative routes for access to Neighbourhood Centres and serve to disperse traffic on a smaller street section.

Avenue/Transit Corridors serve mainly intermediate volumes of intra-neighbourhood/district travel, accommodate local transit, connect Urban Centres Areas and serve as a major internal connector for Urban Core Areas.

The treatment of the boulevard will reflect adjacent land use and whether on-street parking is provided.

Specific technical details of the cross-section (i.e. plant material, soil type, engineering standards) will be determined through the appropriate design review process.

Transit Supportive Uses

Transit supportive land uses are to be encouraged along the right-of-way, such as:

- Walk-up apartments;
- Townhouses; and,
- Small-lot detached homes.



Figure 5.4: Typical Avenue/Transit Corridor section through the General Urban and Sub Urban Design. Street tree height at maturity will vary according to species and the availability of optimum growing conditions (i.e. adequate water, sunlight, soil volume), the protection from compacted soils, salt spray, mechanical damage, pests, and maintenance programs. Please refer to tree habitat design guidelines found in Table 9 of Oakville's Urban Forest: Our Solution to Our Pollution (2006).

5.2.3. Connector/Transit Corridor

Connector/Transit Corridors should be designed to serve relatively low volumes of intra-neighbourhood travel, accommodate local transit service and distribute traffic to and from Major and Minor Arterial/Transit Corridors and Avenue/Transit Corridors.

The treatment of the boulevard will reflect adjacent land use and whether on-street parking is provided.

Specific technical details of the cross-section (i.e. plant material, soil type, engineering standards) will be determined through the appropriate design review process.

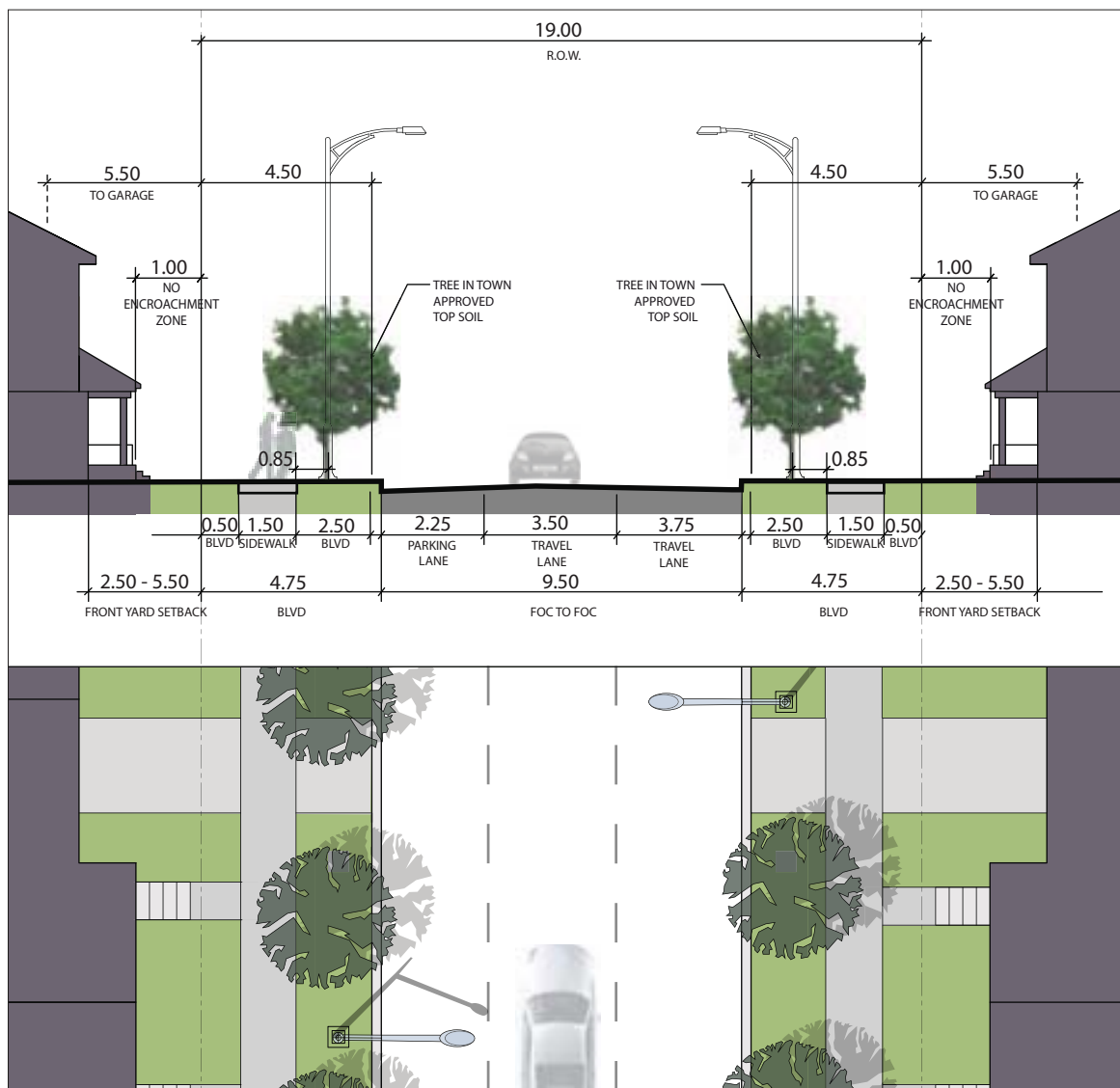


Figure 5.5: Typical Connector/Transit Corridor section through the General Urban and Sub Urban Designation. Street tree height at maturity will vary according to species and the availability of optimum growing conditions (i.e. adequate water, sunlight, soil volume), the protection from compacted soils, salt spray, mechanical damage, pests, and maintenance programs. Please refer to tree habitat design guidelines found in Table 9 of Oakville's Urban Forest: Our Solution to Our Pollution (2006).

5.2.4. Local Road

Local Roads will be designed to provide access to individual properties and serve internal residential neighbourhood, Core Area or Employment District travel demands. Local Roads will also connect individual properties to other Local Roads, Avenue/ Transit Corridors or Connector/Transit Corridors.

The treatment of the boulevard will reflect adjacent land use and whether on-street parking is provided.

Specific technical details of the cross-section (i.e. plant material, soil type, engineering standards) will be determined through the appropriate design review process.

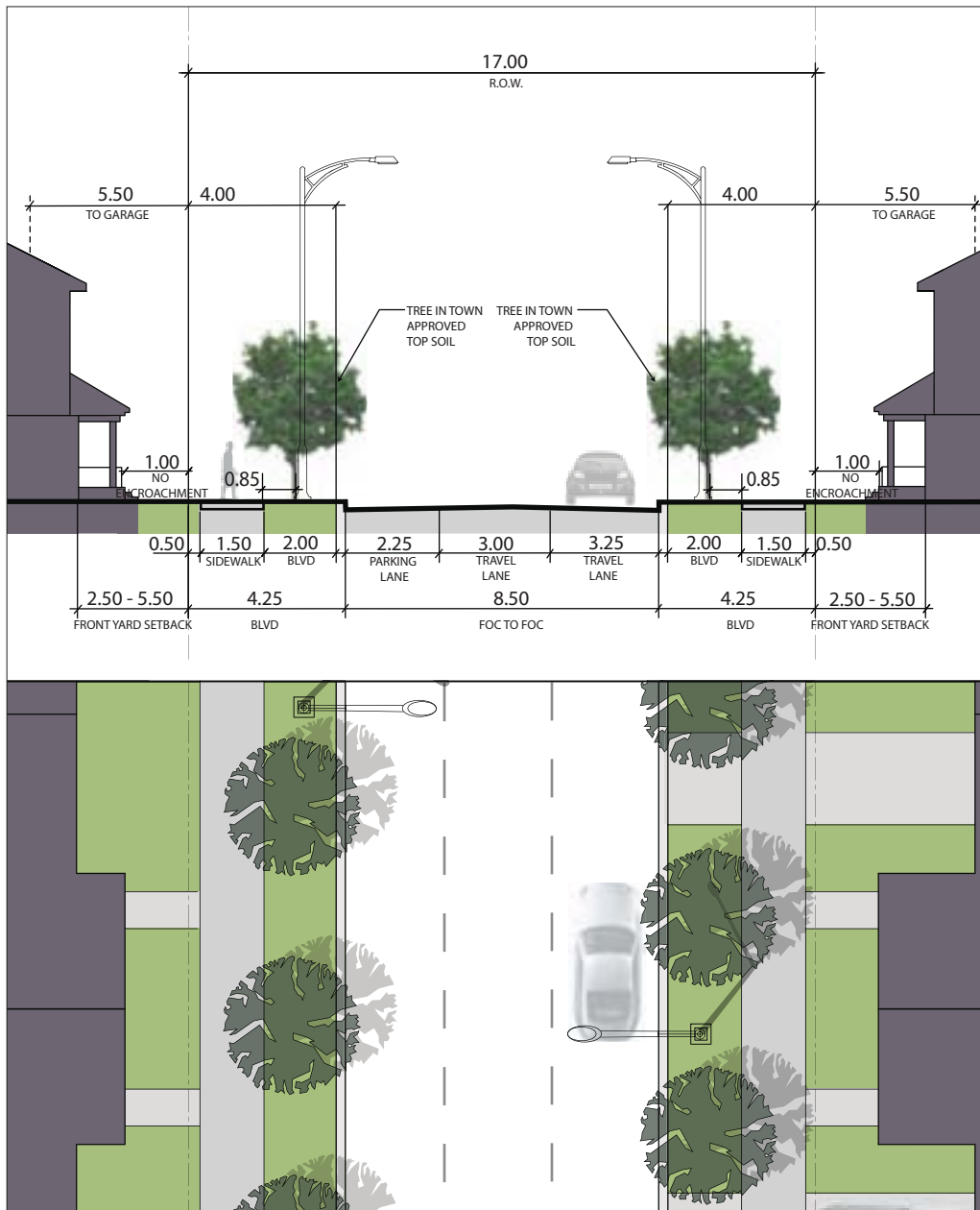


Figure 5.6: Typical Local Road section through the General Urban and Sub Urban Designation. Street tree height at maturity will vary according to species and the availability of optimum growing conditions (i.e. adequate water, sunlight, soil volume), the protection from compacted soils, salt spray, mechanical damage, pests, and maintenance programs. Please refer to tree habitat design guidelines found in Table 9 of Oakville's Urban Forest: Our Solution to Our Pollution (2006).

Laneway

Typical roadway cross-sections for the 11.0m laneway right-of way includes:

- Sidewalks on one side of the laneway;
- One lane in each direction;
- Buffer setback on both sides - may include small stature/narrow crown street tree and foundation planting;
- Street tree species shall adhere to approved Town of Oakville specifications;
- All planting shall be in accordance with the North Oakville Urban Forestry Strategic Management Plan.
- Street light poles and luminaires shall reflect approved Town standards, complementary to the surrounding communities.

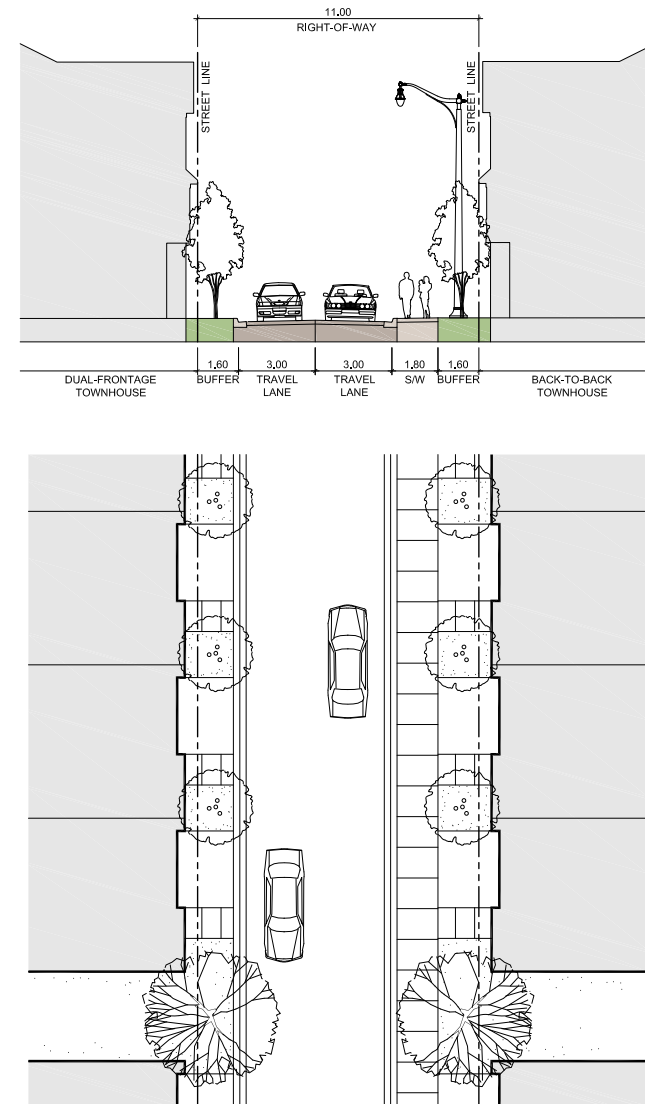
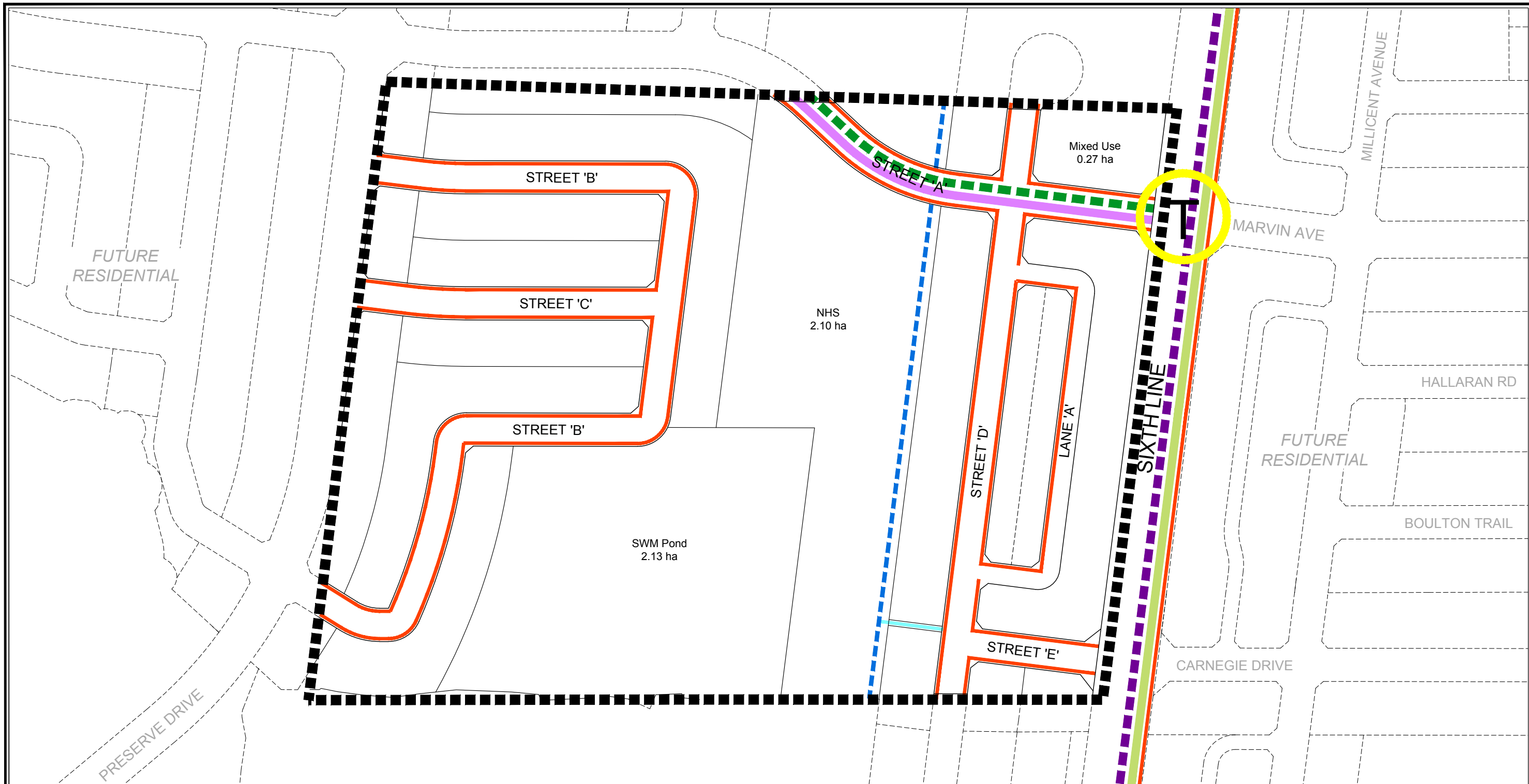


Figure 6.3.1d - Laneway - 11.0m R.O.W. / 2 travel lanes / sidewalk on one side / buffer on both sides

Attachment 4




Korsiak Planning – Pedestrian Circulation Plan

Korsiak Planning – Parking Concept Plan



Legend

-  Sidewalks
-  Walkways
-  Bicycle Lane**
-  Signed Bike Route**
-  Major Trail System**

-  Secondary Transit Corridor Service*
-  Community Service*
-  Potential Transit Stop Location

* From Figure NOE4 - Transportation Plan - NOESP
 ** From Figure 1 - North Oakville Trails Plan

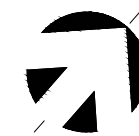
Argo West Morrison Creek

PEDESTRIAN CIRCULATION / TRANSIT FACILITY PLAN

SCALE 1:2000

December 19, 2019

S:\Korsiak & Company\ARGO West Morrison\Pedestrian Circulation\Dec 19\Argo West Morrison - Pedestrian Circulation - Dec 19_se.dwg



206-277 Lakeshore Road East
 Oakville, Ontario L6J 1H9
 T: 905-257-0227
 info@korsiak.com

S:\Korsiak & Company\ARGO\West Morrison\Parking Plan - Dec 19 19_sed.dwg

PRELIMINARY ON-STREET PARKING ANALYSIS

Argo (West Morrison Creek) Limited

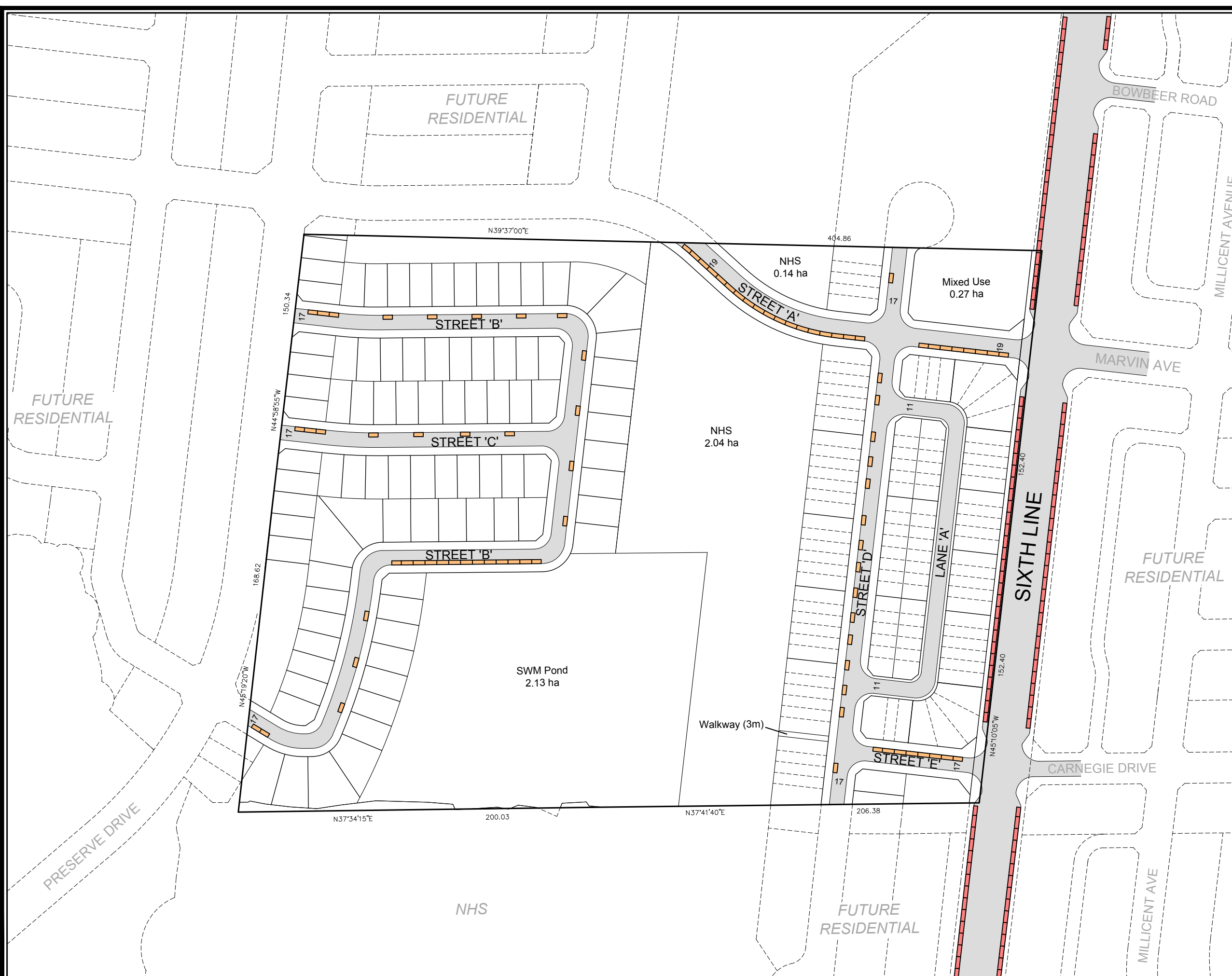
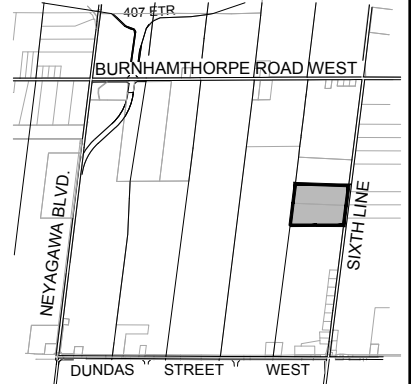
PART OF LOT 16
CONCESSION 1, NORTH OF DUNDAS STREET



GEOGRAPHIC TOWNSHIP OF TRAFALGAR
NOW IN THE
TOWN OF OAKVILLE
REGIONAL MUNICIPALITY OF HALTON

KEY MAP

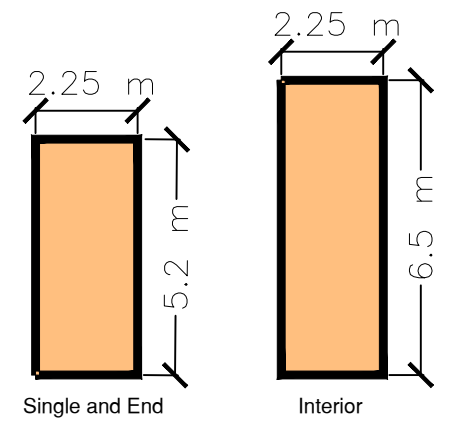
N.T.S.

 SUBJECT PROPERTY



-  ± 87 On-street parking spaces
- ± 206 Residential units
- ± 0.42 Visitor parking space/ unit
-  ± 129 Sixth Line on street parking spaces

TYPICAL ON-STREET PARKING SPACE*



* Per North Oakville Parking Strategy



SCALE 1:2000
December 19, 2019

