

# Heritage Impact Assessment: Conformity with Heritage Conservation District Plan

**Proposed Residential Development  
358 Reynolds Street  
Oakville, Ontario**



Prepared for:

**MacDonald Rose Inc.**

Prepared By:

**Vincent J. Santamaura, Architect Inc.**

08 December 2023  
Project No. 2023-40



# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

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# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

## 1.0 Executive Summary

MacDonald Rose Inc. has purchased the property at 358 Reynolds Street, Part of Lot O, Registered Plan 1, Town of Oakville, Regional Municipality of Halton, Ontario. The property is located on the south side of MacDonald Road at the intersection with Reynolds Street. The Property Owner is proposing an infill Residential Townhouse Development to execute the Town of Oakville's vision for the town.

The property at 358 Reynolds Street is located within the Trafalgar Road Heritage Conservation District (TRHCD). As the property is located within the TRHCD, a Heritage Impact Assessment (HIA) is required to be performed to establish whether the infill townhouse project is in compliance with the TRHCD Plan's policies and guidelines for building within the TRHCD.

MacDonald Rose Inc. has retained Vincent J. Santamaura, Architect Inc., CAHP to prepare a scoped Heritage Impact Assessment to assess the proposed work's compliance with the TRHCD Plan. The HIA performs a review of the proposed design against the policies and guidelines listed in the TRHCD.

Following its review and analysis, Vincent J. Santamaura, Architect Inc., CAHP presents the following Recommendations regarding the compliance of the proposed design with the Trafalgar Road Heritage Conservation District Plan:

With respect to the Infill Residential Townhouse development at 358 Reynolds Street, it is recommended that:

- 1) The existing Building at 358 Reynolds St is considered "Non-Heritage"; has no Heritage value and can be demolished;
- 2) The Infill Residential Townhouse development is in general compliance with the Principals, Objectives and Guidelines of the Trafalgar Road Heritage Conservation District Plan;
- 3) The Infill Residential Townhouse development does not negatively impact the Heritage character of the Trafalgar Road Heritage Conservation District; and
- 4) The Infill Residential Townhouse development be approved.

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

## 2.0 Introduction

### 2.1 Overview

MacDonald Rose Inc. has purchased the property at 358 Reynolds Street, Part of Lot O, Registered Plan 1, Town of Oakville, Regional Municipality of Halton, Ontario. The property is located on the south side of MacDonald Road, at the southwest intersection with Reynolds Street. A 4 storey office building sits on the site with surface parking. The property has existing single detached residential lots on its northwest, and southwest sides; rear yards of residential lots and MacLachlan College campus lands to the southeast, and the former Trafalgar Hospital Lands to the northeast which are undergoing re-development and intensification.

MacDonald Rose Inc. is proposing to construct an Infill Residential development consisting of eleven, 3 storey townhouse units divided into 2 blocks designed in a heritage inspired Architectural style.

The property at 358 Reynolds St. is located within the Trafalgar Road Heritage Conservation District (TRHCD). As the property is located within the TRHCD, a Heritage Impact Assessment (HIA) is required to be performed to establish whether the proposed development is in compliance with the TRHCD Plan's policies and guidelines for new construction within the TRHCD.

The Property Owner has retained Vincent J. Santamaura, Architect Inc., CAHP to prepare a scoped Heritage Impact Assessment to assess the proposed work's compliance with the TRHCD Plan.

### 2.2 Associated Documents

This Heritage Impact Assessment (HIA) has been prepared with regard to the following governing documents:

- Provincial Policy Statement,
- *The Ontario Heritage Act, R.S.O. 1990, c.o.18*
- The Halton Region Official Plan, 2021,
- The Livable Oakville Plan, 2009, updated 2021,
- Town of Oakville Zoning By-law 2014-014, as amended
- Town of Oakville, Trafalgar Road Heritage Conservation District Plan, 1994,
- Town of Oakville, Heritage Impact Assessments Terms of Reference, 2017,
- The Ontario Building Code 2012,
- Comments by The Town of Oakville Heritage Department from a pre-consultation meeting dated March 29<sup>th</sup>, 2023.

As a scoped HIA, the report will focus solely on the Policies and Guidelines in the Trafalgar Road Heritage Conservation District Plan.

# Heritage Impact Assessment

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## 3.0 Present Owner Contact Information:

MacDonald Rose Inc.  
145 Reynolds Street, Suite 400  
Oakville ON.  
L6J 0A7

## Site Documentation

### 4.1 Site Inventory

#### 4.1.1 Site Location – 358 Reynolds Street

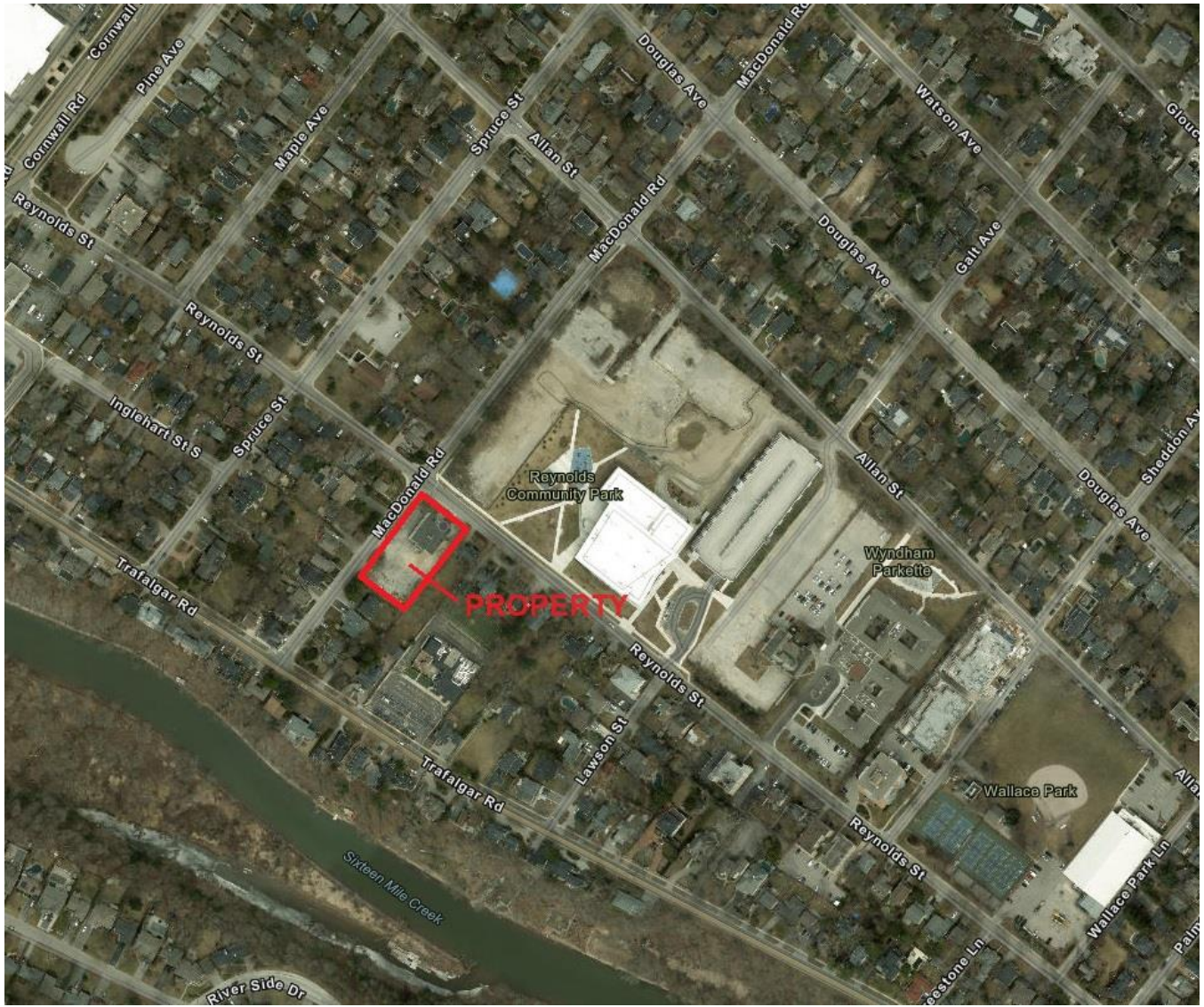
The property is located on the northeastern part of the early Trafalgar Road area on the south west corner of MacDonald St. and Reynolds St. The lot dates back to the early 1860's and was on the north edge of the early fabric of the village at that time. It remained vacant until after the construction of Oakville Trafalgar Memorial Hospital on the property to the northeast across Reynolds St. in 1950. The 4 storey building, itself, was built in 1954, with its use being tied to the hospital. In 1957, it was advertised as the Medical Arts Building.

The lot has an area of approximately 0.283 ha. (0.70ac.). It's northwesterly frontage along MacDonald St. is approximately 76.26m. It has a northeasterly frontage along Reynolds St. of approximately 36.96m, a southeastern property line of approximately 76.18m, and 37.22m along its southwestern property line. The property has existing single detached residential lots on its north, and west sides; rear yards of residential lots and MacLachlan College campus lands to the south, and the former Oakville Trafalgar Hospital Lands to the east which are now undergoing re-development and intensification with the Oakville Trafalgar Community Centre and Reynolds Community Park now built.

A 4 storey office building sits on the site with surface parking. It is constructed with concrete floor and masonry walls. It has been renovated a few times and currently the exterior brick has been painted. To the rear of the building is a paved parking area.

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4.1.1.A - Aerial Photograph - Context

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario



4.1.1.B - Aerial Photograph – Property

# Heritage Impact Assessment

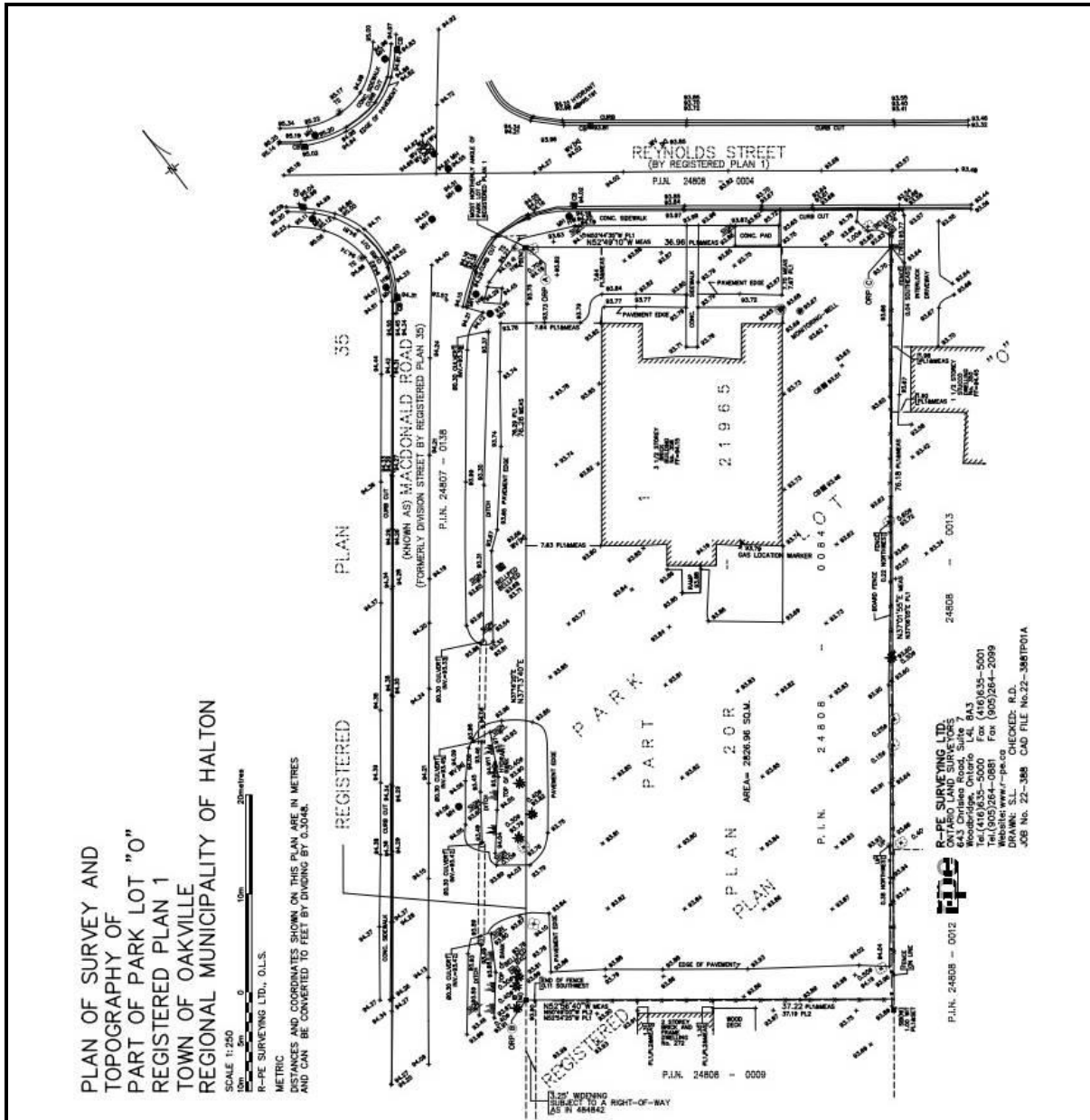
Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

## 4.1.2 Site Identification:

The parcel of land is defined as:

Part of Lot O,  
Registered Plan 1,  
Town of Oakville,  
Regional Municipality of Halton, Ontario.

The lot is addressed as: 358 Reynolds Street



### 4.1.2.A - Survey – Topographical



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## 4.1.3 Current Applicable Designations:

The Legislation and Authorities having jurisdiction below may override Heritage concerns and recommendations included in this Heritage Impact Assessment. The lot is currently designated as follows:

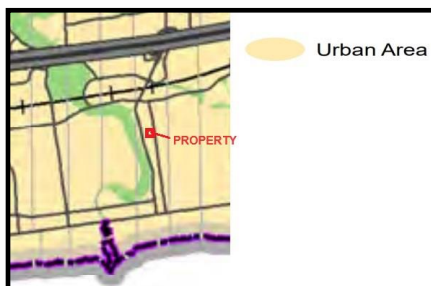
### 4.1.3.1 Conservation Halton:

- Non-Regulated Area



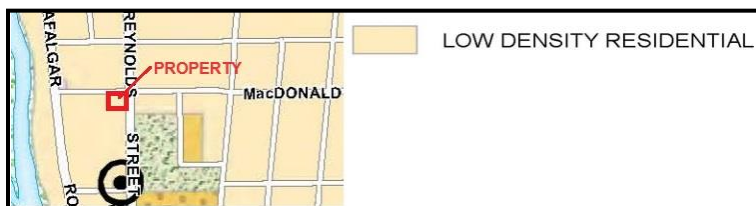
### 4.1.3.2 Region of Halton Official Plan 2021:

- Urban Area



### 4.1.3.3 Oakville Official Plan:

- Medium Density Residential (Site Specific exception 416 as per OLT)



# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

## 4.1.3.4 Town of Oakville Zoning By-Law 2014-014

- RM4 sp:416



## 4.1.3.5 Heritage Designations:

### 4.1.3.5.1 Town of Oakville Heritage Inventory:

358 Reynolds Street– Not Listed;

### 4.1.3.5.2 Ontario Heritage Act, Part V-

Town of Oakville List of Designated Buildings:

358 Reynolds Street– Not Listed;

### 4.1.3.5.3 Ontario Heritage Act, Part IV -

Trafalgar Road Heritage Conservation District Plan - 2014:

358 Reynolds Street – Designated, considered “Non-Heritage”  
(built after 1952)

# Heritage Impact Assessment

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## 4.1.4 Site Access

The property fronts onto Reynolds St with a flanking exposure to MacDonald Rd. Reynolds St. runs north-south, one block east of Trafalgar Rd and Sixteen Mile Creek. The building sits approximately 7.64m back from the Reynold’s street line, and 7.64m back from the MacDonald Rd frontage. It is approximately 1.0 meter closer to the street lines than its neighbouring homes’ front walls. It has an existing southeast interior side yard setback of approximately 10.49m and a southwesterly rear yard setback of about 45.35m. An existing, double width, asphalt driveway leads west from Reynolds St. along the southeastern side of the building to a parking area behind the building. Two driveways lead directly to the parking area from MacDonald Rd.



4.1.4.A - Driveway Access – Reynolds St.



4.1.4.B – Driveway Access – MacDonald Rd.

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4.1.4.C - Approach from the Southeast



4.1.4.C - Approach from the north to Northeast

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4.1.4.D – Approach from Northwest



4.1.4.E -Approach from Southwest

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## 4.1.5 Existing Buildings & Structures

The buildings on the property at 358 Reynolds St. consist of a 4 storey office building with its principal façade facing northeast, onto Reynolds St.

### 4.1.5.1 The Office Building

The building is a 4 storey office building with half of a storey set into the ground. It is of concrete frame construction clad with infill masonry walls and a flat roof. It has a recessed front entrance with a ramp leading up to it. The windows are made of aluminum. There is a recent decorative motif on the central portion of wall above the entry

The ground floor overall dimensions 9.75m wide x 11.14m deep (32'0" x 36'-5"). The existing ground floor area of the home is 99.00m<sup>2</sup> (1065 sf).



4.1.5.1.A - Existing Northeast (Front) Elevation

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4.1.5.1.B – Existing Right (Northwest) Elevation (Left Elevation similar)



4.1.5.1.C – Existing Rear (Southwest) Elevation

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4.1.5.1.D - Existing Front (Northeast) Elevation – 2015



4.1.5.1.E – Existing Front (Northeast) Elevation – 2011



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## 4.1.6 Local Neighbourhood Character

The Trafalgar Road Heritage Conservation District contains portions of the Old Town of Oakville. The first portion of the Town was laid out in 1835 with Palmer Avenue as the northern limit of the lotting plan. Upon the arrival of the Hamilton & Toronto Railway in the 1850's, Trafalgar Rd (know then as Dundas St) was then extended north to the Railway station and depot. The second phase of expansion of the Town was northward to Spruce St. Being away from Sixteen Mile Creek, this portion of the Town developed mostly as a residential district.

In late 1940's, the large property to the (north)east of the property across Reynolds St. was the remanent of the Chislom estate which was donated to build the Oakville Trafalgar Memorial Hospital. The hospital was officially opened in 1950.

At this time, the property at 358 Reynolds St. was still sitting vacant. Given the change in local character, the property was developed to provide support services to the hospital. A 4 storey office building in a simple Modernist Architectural style was built in 1954 which eventually became known as the Medical Arts Building. Upon the demolition and relocation of the hospital in 2017, the building was used as clinic and medical offices, but without its context of being related to the hospital. The former hospital site is now being developed as a mixed use community and now contains the Oakville Trafalgar Community Centre and Reynolds Park facility with mid-rise and multi-unit townhouses proposed. The former hospital site is not within the TRHCD.

The surrounding residential component of the neighbourhood originally consisted of single detached dwellings, but is gradually intensifying with multi-unit townhouses and low-rise apartments.

The fabric of the neighbourhood consists of a single detached dwellings and multi-unit apartments on a variety of lot widths. Building setbacks vary with the age of the building – the older ones being closer to the street, and the more recent ones set further back. Front yards vary from 4.0m to 13.00m +. Depending on the lot width, interior side yards vary from 1.20m for narrower, earlier built dwellings to 2.40m or greater for wider lots with dwellings built more recently. Building height varies from 1 to 4 storeys with flat, hipped and gable roof lines. Exterior cladding materials range from siding to masonry to stucco and stone.

Of particular note is the property at 293 MacDonald Rd. on the north side of the street from the subject property. The property is excluded from the TRHCD, but is designated under Part IV of the Ontario Heritage Act (Oakville By-law No. 1984-153). Know as the Campbell House, it was the residence of Donald Campbell, a soldier who served with The Argyle and Sutherland Highlanders who settled in Oakville following his discharge in 1842. He purchased the lot form John Chisholm in 1856, building the home in 1857.

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(Designated Campbell House on right)



4.1.6.A - MacDonald Rd – north side from property

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4.1.6.B - Reynolds St. – South of Property



4.1.6 C - Reynolds St. Adjoining TRHCD

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

## 5.0 Proposal Description

### 5.1 Scope of Work:

MacDonald Rose Inc. proposes an infill Townhouse development to execute the broader vision and goals of the Town of Oakville's Official plan; be sensitive to the Heritage character of the TRHCD and follow TRHCD policies and guidelines.

The design solution begins by proposing the demolition of the existing office building which has no heritage value, and whose built form, use and volume are not in keeping the heritage character of the neighbourhood.

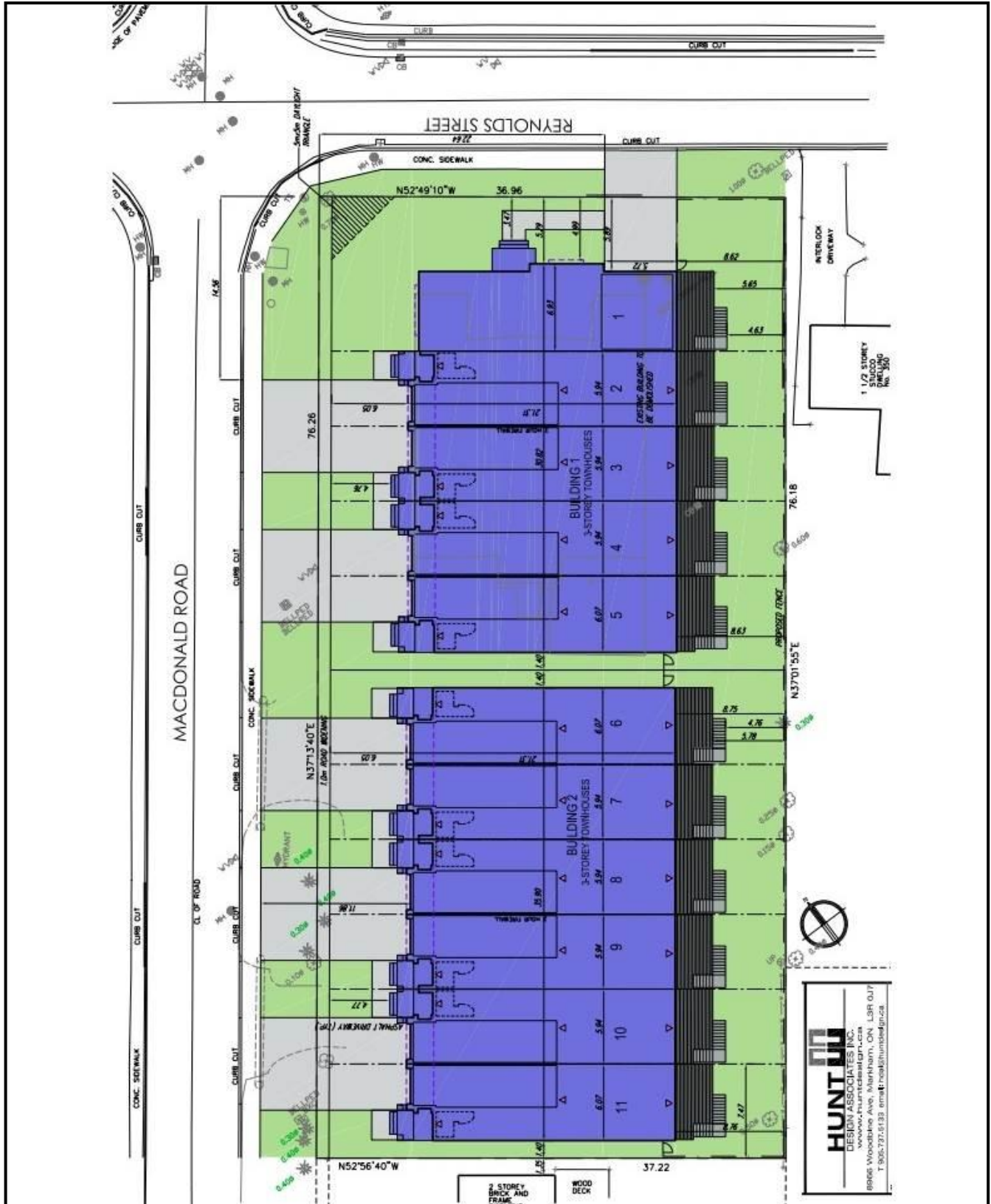
Two blocks of 3 storey townhouses with integral garages facing MacDonald Rd are proposed designed in a heritage inspired Architectural style. Driveway access for each individual unit will be from MacDonald Rd. Rear yards will be facing the southeast. Front yard setbacks are proposed at 6.00m to the garage and main wall of the units. The buildings will be 21.31m long with rear yards at a minimum of 8.63m from the rear wall face. Decks at second storey off the living area are proposed to encroach into the rear yard 2.0m.

The Townhouse blocks have been divided into groupings of 5 and 6 units with internal side yards of 1.40m. The flankage unit facing Reynolds St will have its entry and driveway facing Reynolds St., with the flanking elevation facing Reynolds St. being architecturally enhanced to the design level of the front elevations. Exterior cladding materials are proposed to be di-chromatic masonry with quoining and soldier course window headers and belt coursing. Units will be paired to create a wider visual appearance with truncated hip shaped roofs to visually reduce the building height. Hipped roofs along the side will reduce the scale and mass of the building beside the adjoining properties.

For larger drawings, refer to the Architectural Drawings package submitted with the application.

# Heritage Impact Assessment

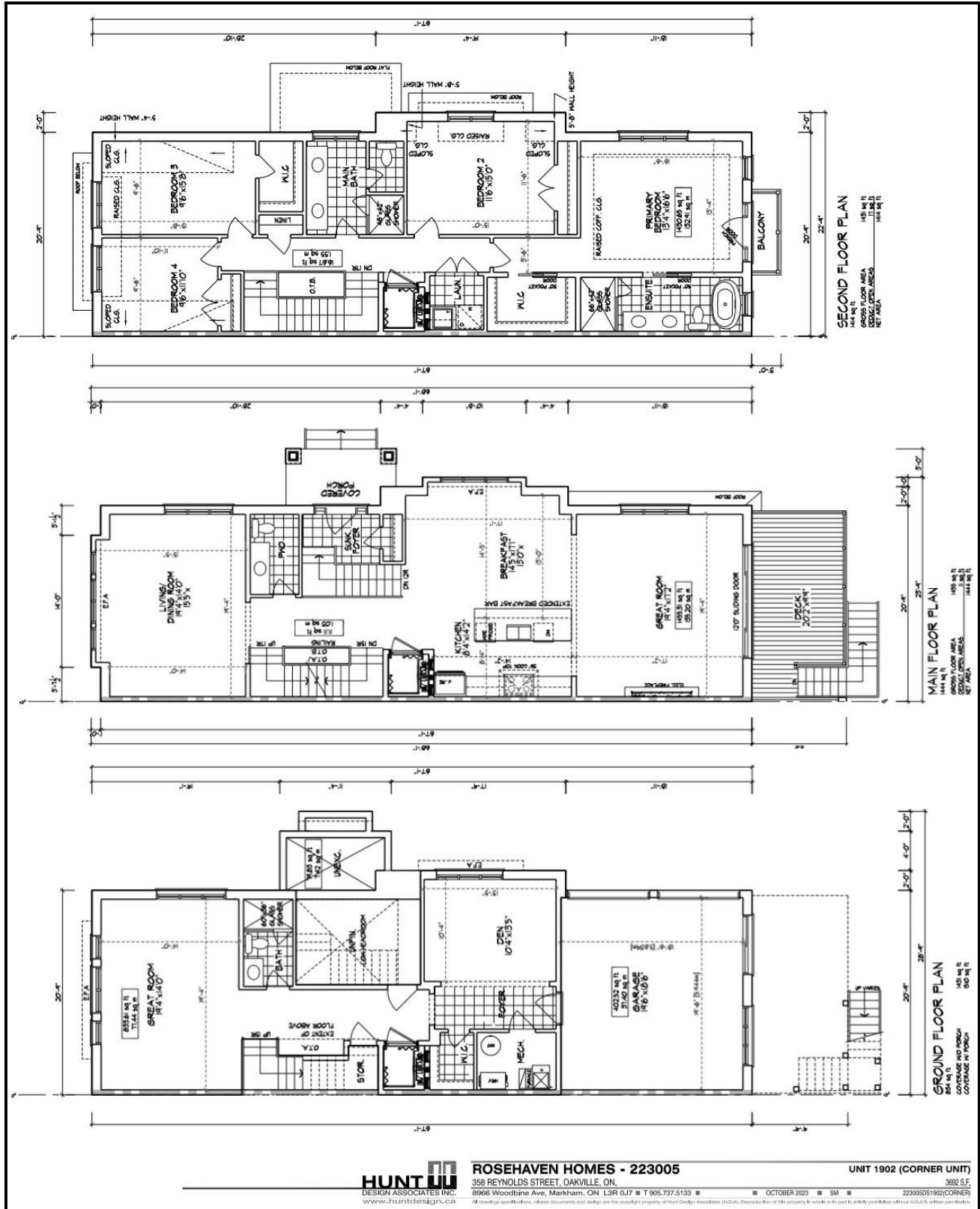
Proposed Residential Development, 358 Reynolds Street, Oakville Ontario



5.1.A – Proposed Site Plan

# Heritage Impact Assessment

## Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

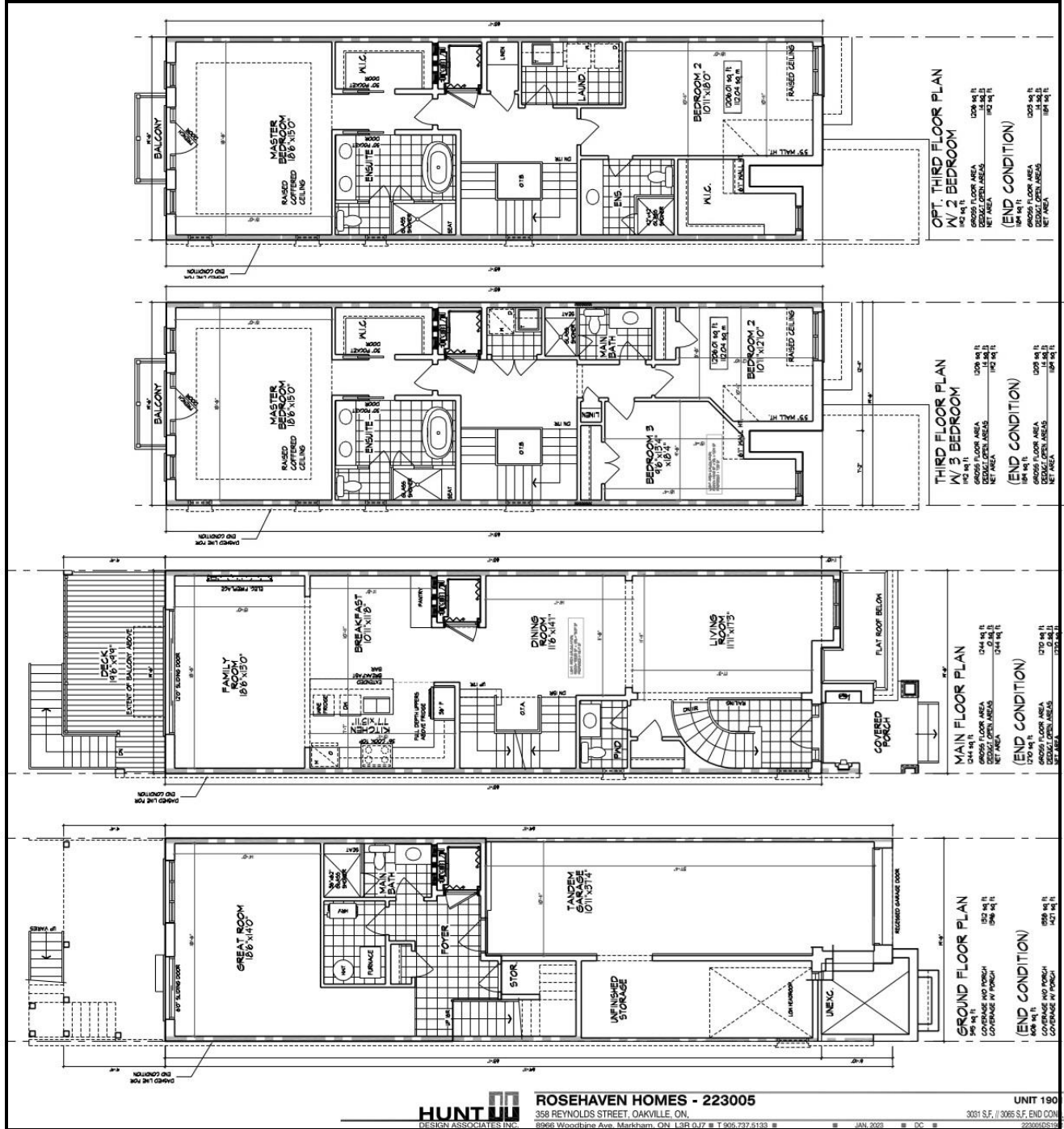


5.1.B – Proposed Flankage Unit Floor Plans

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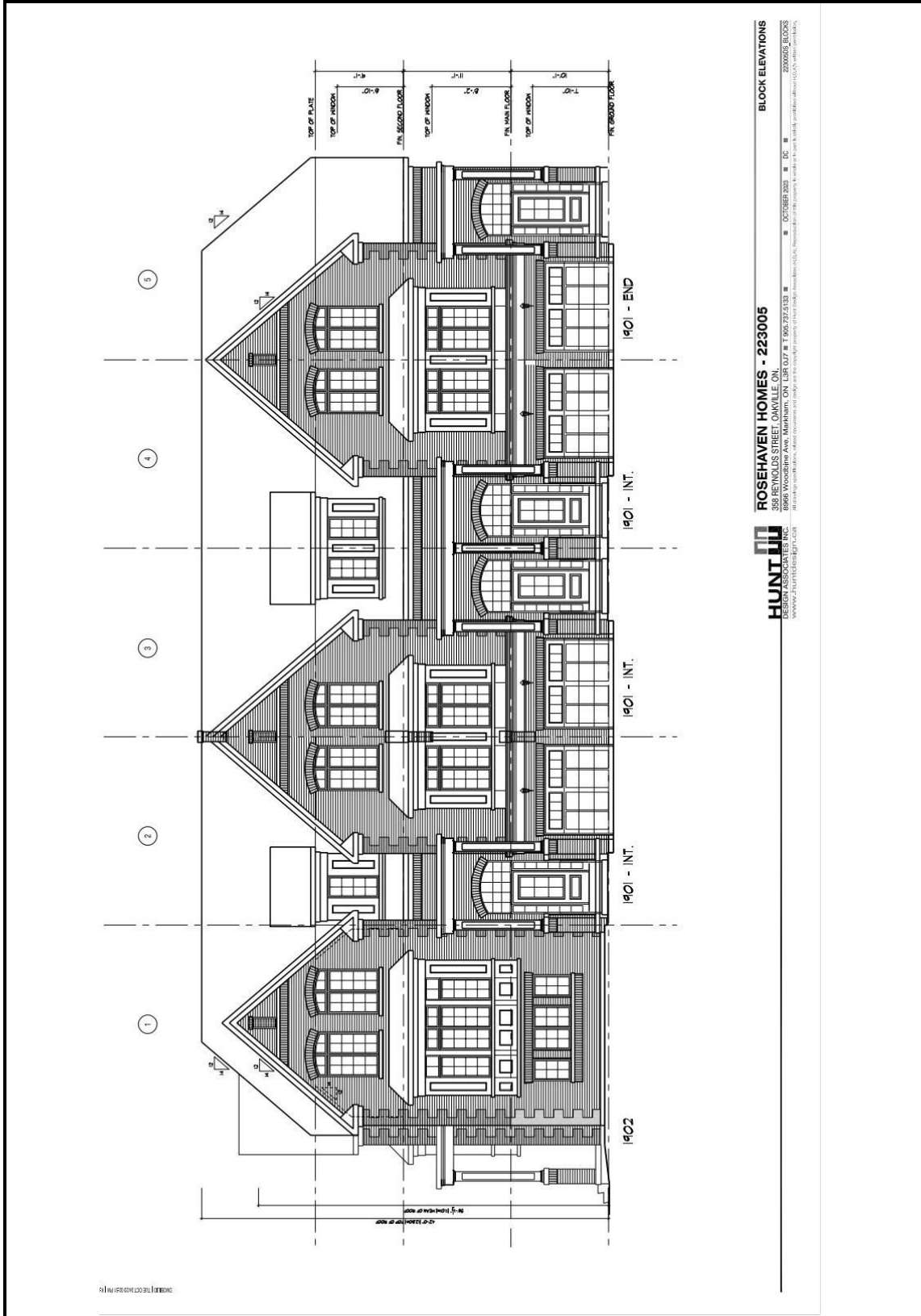
Proposed Residential Development, 358 Reynolds Street, Oakville Ontario



5.1.C – Proposed Interior Unit Floor Plans

# Heritage Impact Assessment

## Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

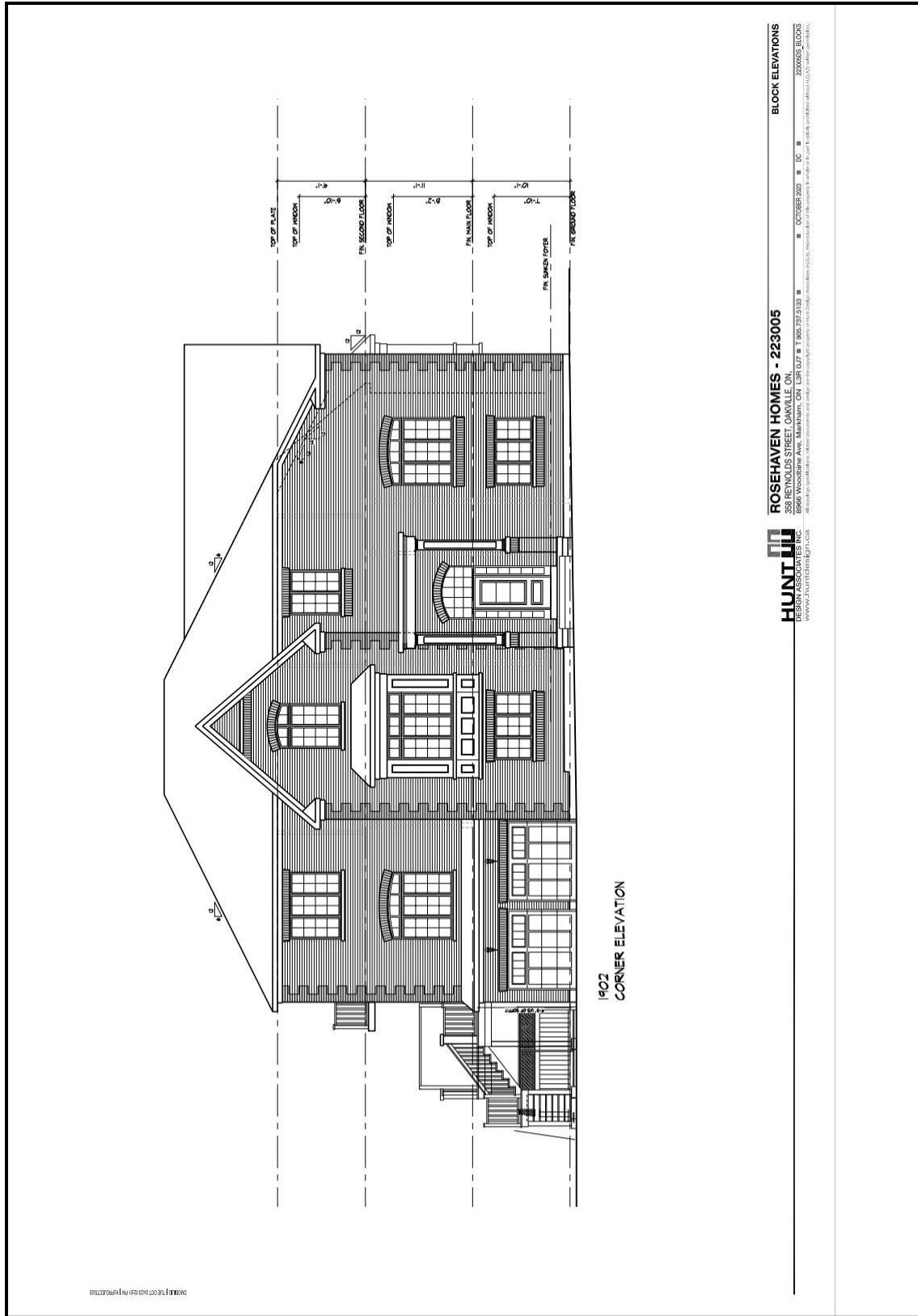


5.1.D – Building 1 – Proposed Front Elevation – (MacDonald Rd.)



# Heritage Impact Assessment

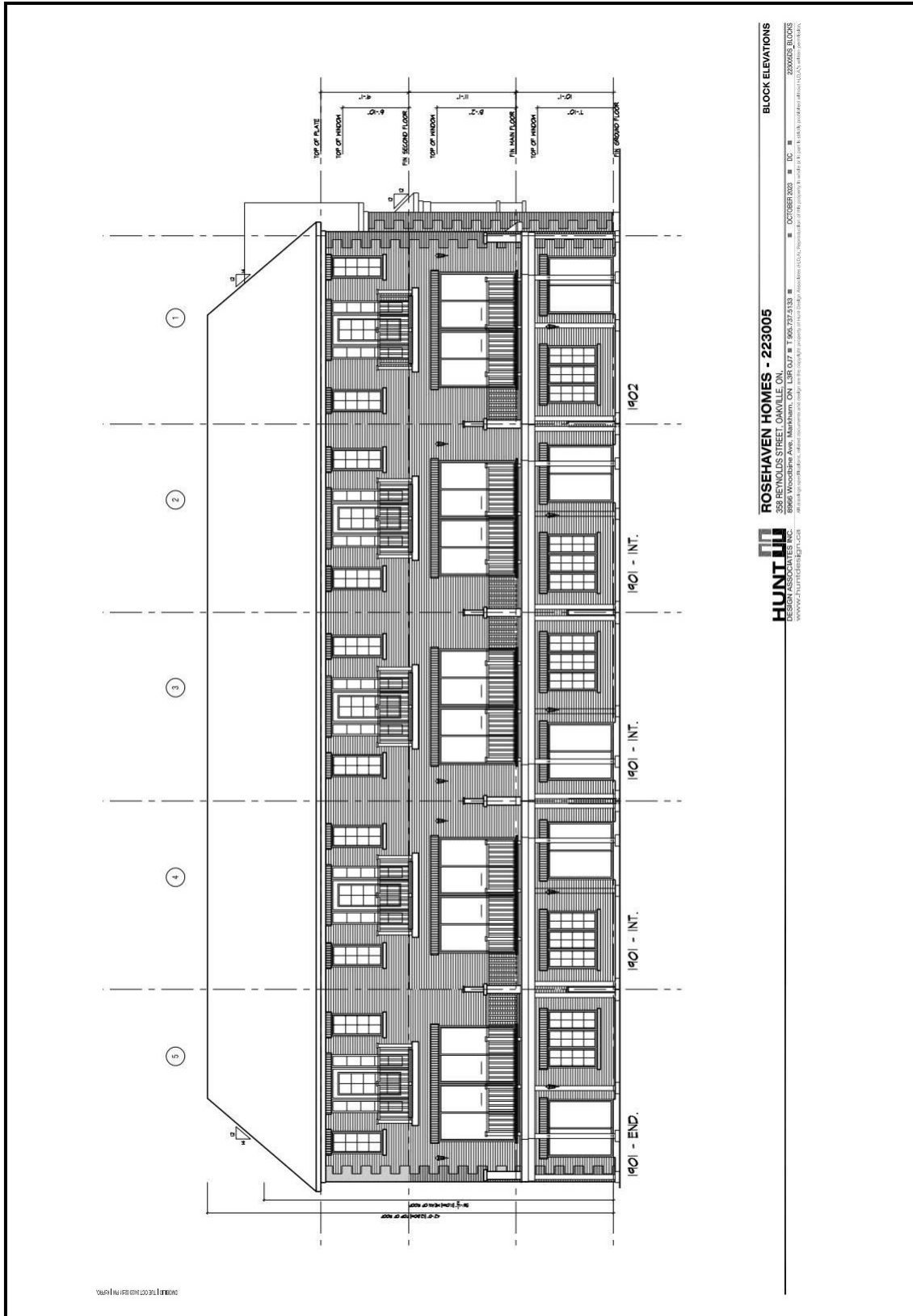
Proposed Residential Development, 358 Reynolds Street, Oakville Ontario



5.1.E – Building 1 - Proposed Left (Flankage) Elevation (Reynolds St.)

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## Proposed Residential Development, 358 Reynolds Street, Oakville Ontario



5.1.F – Block 1 - Proposed Rear Elevation

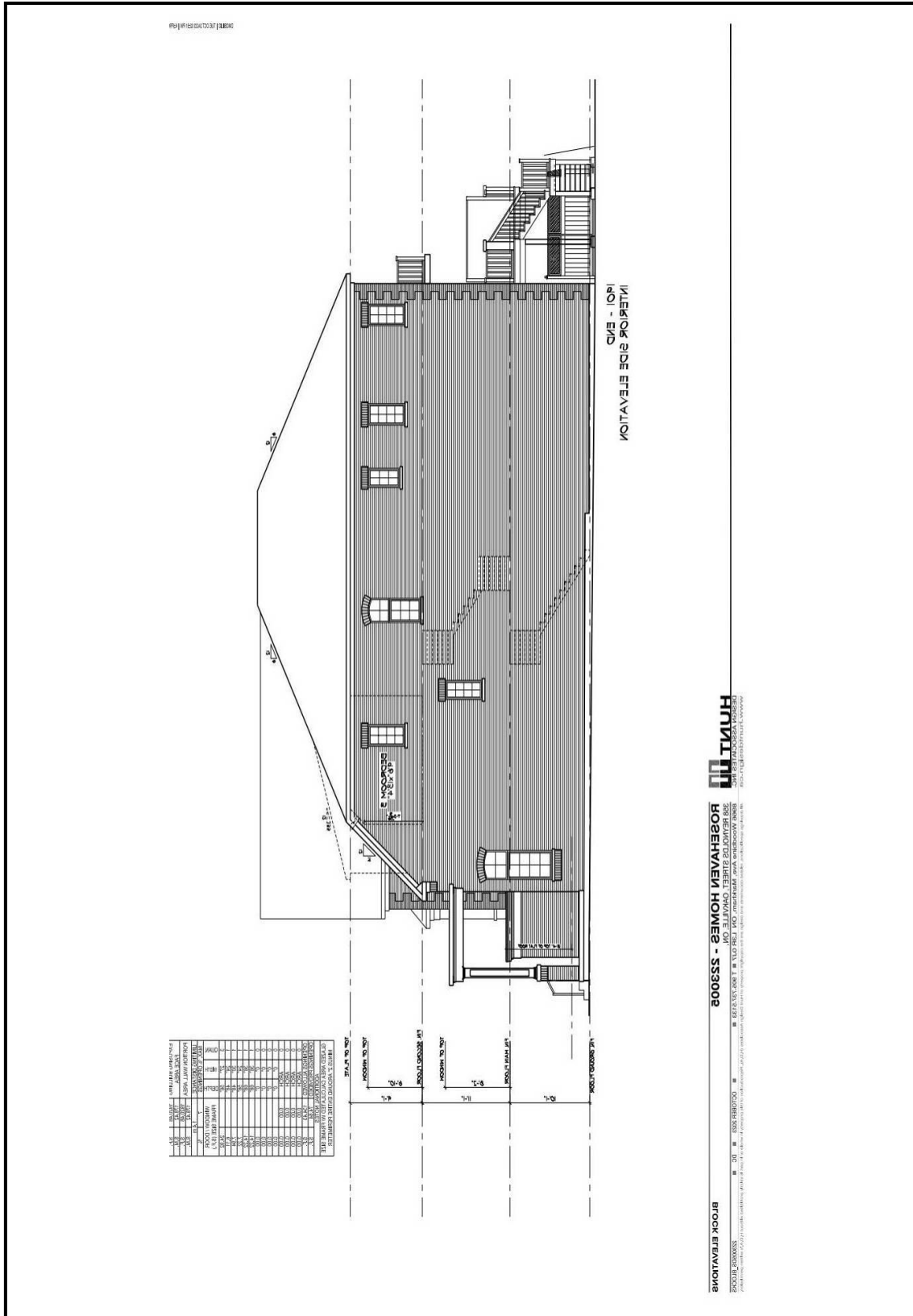
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**BLOCK ELEVATIONS**  
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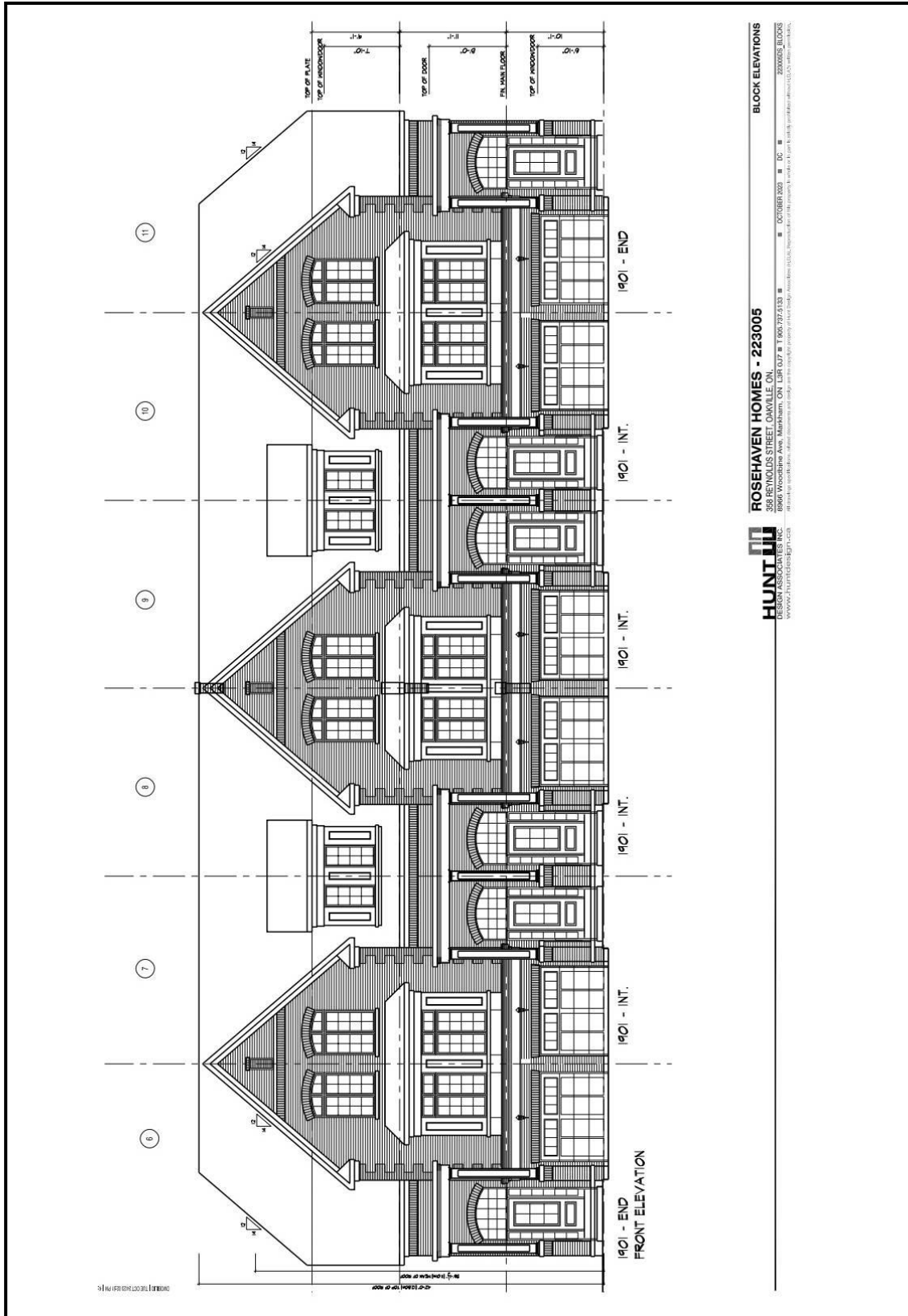
Proposed Residential Development, 358 Reynolds Street, Oakville Ontario



5.1.G – Building 2 - Proposed Right (Interior) Elevation

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5.1.H – Building 2 - Proposed Front Elevation (MacDonald Rd.)





# Heritage Impact Assessment

## Proposed Residential Development, 358 Reynolds Street, Oakville Ontario



5.1.J – Building 2 - Proposed Rear Elevation

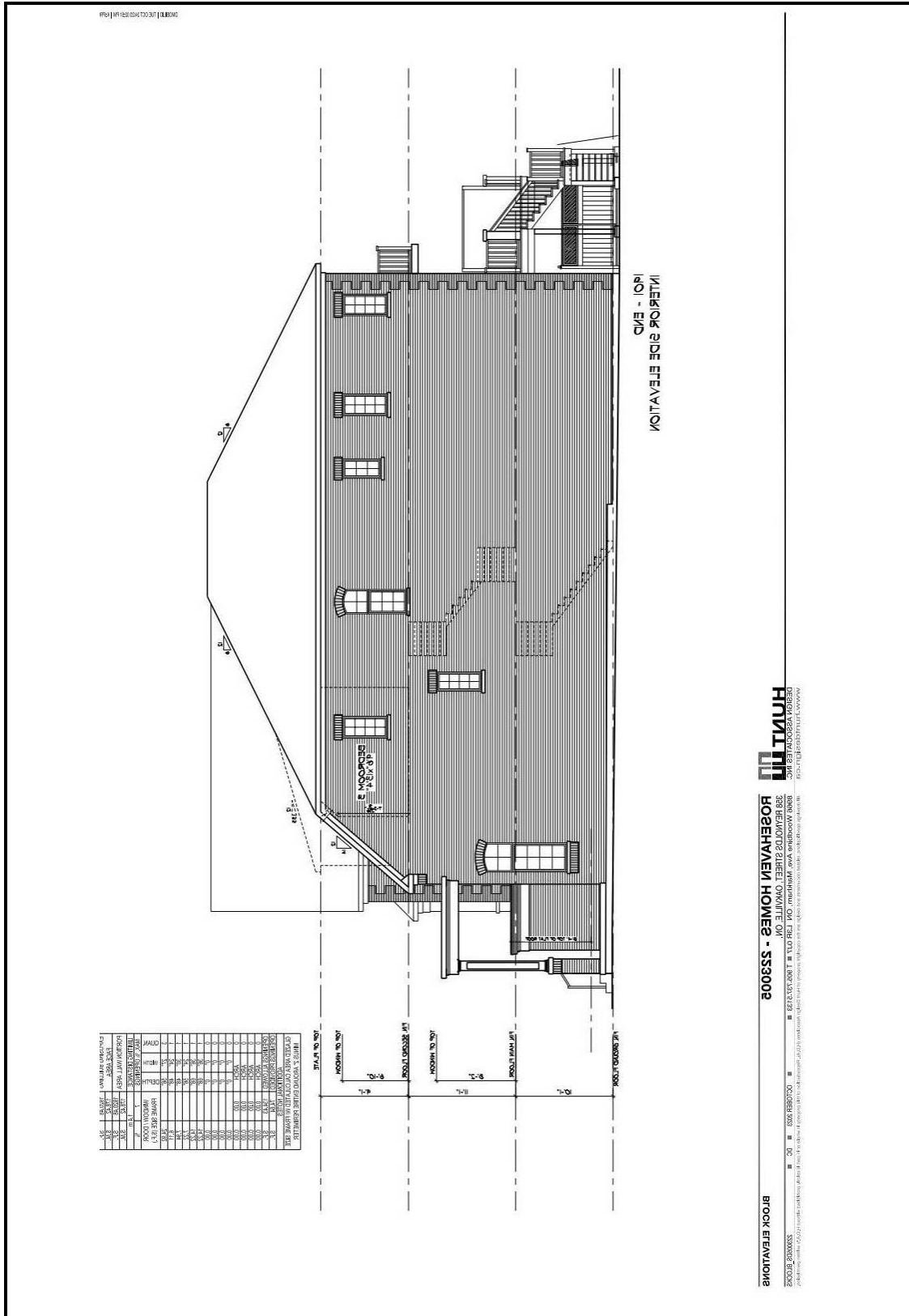
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 DATE: OCTOBER 2023 | ID: 223005\_BLDG2

# Heritage Impact Assessment

## Proposed Residential Development, 358 Reynolds Street, Oakville Ontario



5.1.K – Building 2 - Proposed Right Elevation

# Heritage Impact Assessment

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5.1.L – Building 1 – 3D Massing (Reynolds St.)



5.1.M – Building 1 – 3D Massing (Reynolds St. & MacDonald Rd.)



# Heritage Impact Assessment

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## 5.1.N – Building 1 & 2 – 3D Massing (MacDonald Rd.)

The preliminary material palette consists of the following:

Rosehaven Homes - HERITAGE TOWNHOUSES PROPOSED EXTERIOR COLOURS - 358 REYNOLDS ST.		
EXTERIOR COLOUR SELECTIONS	MANUFACTURER	COLOUR
Roof (Main field colour)	CERTAINTEED – Landmark or BP – Mystique	Moire Black
Metal Roof (if applicable)	Colour Steels (or equivalent)	Black
Main Brick	Brampton Brick	Crawford
Accent Brick	Brampton Brick	Canyon
Stone Plinth	Permacon Lafitt Stone	Range Chambord Grey
Aluminum soffit/fascia	Kaycan	Mocha
Trim – all exterior painted wood columns	Para Paints or Sherwin Williams	Kittiwake Grey P2632-1
Hardie Board Panels at Boxout Bay Windows	James Hardie	Cobblestone
Hardie Board Frieze	James Hardie	Cobblestone
Front Door	Old Masters (Gel Stain)	Dark Mahogany
Garage Door – Barmac Manor House Collection	Para Paints or Sherwin Williams Equivalent	Bramble Tan P2332-1
Windows & Doors	Casa Bella (By Newmar)	White
Interlock Driveway	Oaks - Ridgefield Plus	Cedar Blend
Landscape Porch Steps	Oaks - Fieldstone	Silversand
<b>Notes:</b>		
1) Roof Vent/Flashing – Paint to match roof colour		
2) Decorative railings to be metal black		
3) Basement windows to be same colour as 1st and 2nd floor		
4) Front door hardware (supplied by Newmar) – Toronto Series (M1020/374N) Matte Black Lever on outside and inside		
5) Front Door (supplied by Newmar) – Fibreglass Mahogany Grain 36" x 96" (MAH-8'NRP)		

## 5.1.O – Colour Package - Preliminary

# Heritage Impact Assessment

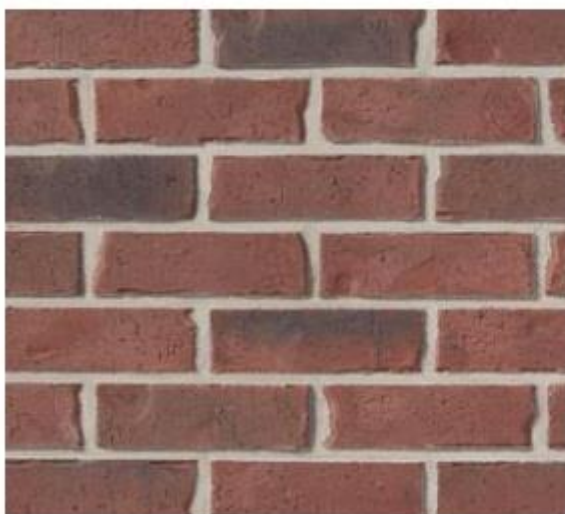
Proposed Residential Development, 358 Reynolds Street, Oakville Ontario



5.1.P – Colour Package-Preliminary Sample Board

Field Brick = CRAWFORD  
(Brampton Brick - premier plus size)

Crawford



Accent Brick = Canyon  
(Brampton Brick - premier plus size)

Canyon



5.1.P – Colour Package – Preliminary Brick Colour Samples

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5.1.Q – Colour Package – Concept Materials rendering



5.1.R – Building 1 & 2 – As-Built Sample

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario



5.1.S – Proposed Landscape Plan

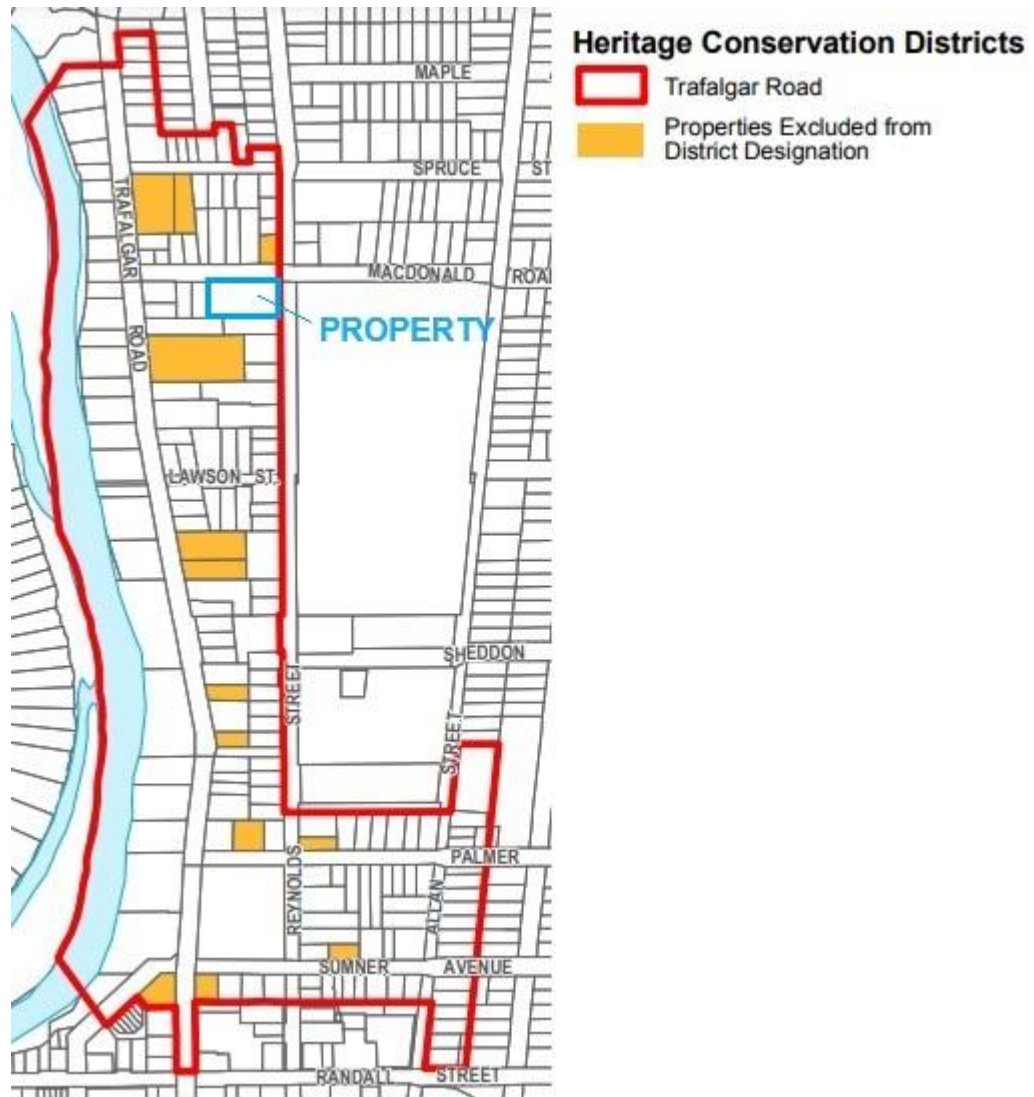
# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

## 6.0 Heritage Impact Assessment

### 6.1 Applicable Trafalgar Road Heritage Conservation District Plan Policies and Guidelines

In an effort to preserve the historic character one of the town’s early villages, the Town of Oakville enacted the Trafalgar Road Heritage District Conservation Plan in 1994. While the TRHCD Plan encourages the preservation of as much of the Heritage value of the community as possible, it recognizes that communities are constantly growing and evolving. Foreseeing this activity, the TRHCD Plan has policies and guidelines to guide proposed work needing to be done on existing and new buildings.



6.1.A – Trafalgar Road Heritage Conservation District Boundaries

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

6.1.1 - Conservation Goals, Objectives Principles and Practice are stated in Section 1.3:

*Accordingly any proposed changes within the Trafalgar Road Heritage Conservation District shall be considered with regard to the following principles:*

- *All heritage conservation work should be based upon and preceded by sufficient historical research, site analysis and documentation to identify and safeguard fully the heritage values to be conserved.*
- *There shall be a presumption in favour of retaining the distinguishing characteristics of a heritage property. The destruction, alteration or removal of historical fabric or distinguishing architectural features and landscaping shall be considered as the least desirable course of action.*
- *Alterations and changes that have occurred in the past may be of significance in the development of a particular heritage building and its environment and should be protected.*
- *Heritage features are to be retained and re-used wherever possible and the demolition of heritage buildings shall be actively discouraged.*
- *Historical, physical or pictorial and documentary evidence shall guide the repair or replacement of missing architectural features of an individual heritage building. Guesswork or the use of architectural elements borrowed from other buildings should be avoided.*
- *Stylistic and architectural features or examples of fine craftwork that distinguish a particular building, whether of vernacular construction or more formal architecture, must be treated with sensitivity and where deteriorated should be repaired rather than replaced.*
- *Replacement of architectural features should match the material being replaced in composition, design, texture, colour, size and level of craftwork.*
- *Surface cleaning of historic structures must only be undertaken when accumulated dirt adversely affects the historical fabric of a heritage building and undertaken only by the gentlest means possible. Sandblasting, high pressure water washing, strong chemical cleaning and other methods that may damage building materials must be avoided.*
- *Contemporary design of alterations and additions will be permitted where they do not destroy significant historical, architectural, streetscape or cultural features (See Design Guidelines, Section 4).*

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- *Contemporary design or alterations and additions should be permitted where they are of a size, location, colour and material that is compatible with the prevailing character of the building, streetscape, landscape and district.*
- *All public works should seek to avoid adverse effects to the character of the Trafalgar Road heritage conservation district and in particular to individual heritage buildings, archaeological sites, fences, grass boulevards and distinctive trees. and treelines within the 'district.*
- *New construction comprising freestanding buildings should respect the prevailing character of: adjacent buildings; the existing streetscape, landscaping and grade levels; and the district as a whole. New construction should be of compatible design in location, size, height, setback, orientation, materials, colour, roof and roofline, fenestration, scale and proportion.*
- *Historical landscaping should be respected as an integral part of the character of the district and preserved whenever possible.*

6.1.2 – Conservation Goals and Objectives specifically for the Trafalgar Road Heritage Conservation District for new development are stated in Section 2.2.5.

- *To permit new development where it respects or otherwise complements the prevailing character of existing heritage buildings and structures within the Trafalgar Road District.*

6.1.3 – Guidelines for Alterations, Additions and New Construction are listed in Section 4.0.

*Section 4.1: Physical change that may be expected to occur within the Trafalgar Road District can be categorized by:*

- *alterations and additions to existing structures;*
- *new construction, either through infill or redevelopment; and,*
- *public works.*

*Section 4.2: Within the Trafalgar Road Heritage Conservation District, 118 buildings are considered to have been constructed prior to 1952 and therefore of varying degrees of heritage value and interest. It is the intent of this plan that these existing heritage structures be retained.*

*Section 4.5:*

*4.5.1 General Principles: The general factors that govern the visual relationships between an infill building and its neighbours: height, width, proportion, relationship to the street, roof forms, composition, proportion of openings, materials and colour, should be studied carefully and used as a basis for new construction.*

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

## 4.5.2 New building height:

*One-and-a-half to two storey structures are the most dominant in the district. Building height of new structures should generally maintain the building height of adjacent properties and the immediate streetscape and should be neither noticeably higher nor lower. In areas of varied building height new development must respect adjacent buildings by being neither excessively higher nor lower.*

## 4.5.3 Width:

*The majority of the buildings in the district are single detached dwellings of varying width dependent upon the lot size and stylistic derivation. Building width of new structures should maintain the building width and side yard spaces of adjacent properties and the immediate streetscape thus preserving the existing building and space rhythms within the streetscape.*

## 4.5.4 Proportion:

*Proportion relates to the association of height to width. The structures in the Trafalgar Road district are generally oriented vertically, i.e., the height is greater than the width. New residential infill should maintain the proportions of neighbouring properties.*

## 4.5.5 Relationship to the Street:

*There are a variety of residential building forms in various styles and arrangements within the district. There is no one predominant building line or setback that distinguishes the district. New residential infill should maintain the existing setbacks of adjacent properties. In locations where there is significant variation in setbacks, infill development should generally avoid excessive setback from or projection in front of a building line drawn from the mid-point of adjacent building facades.*

*In the case of corner lots, orientation of the principal elevation to the more major street is generally preferred.*

*Garages in particular should not be a dominant element of the main elevation. They are best located to the rear of the building or set back from the principal facade.*



# Heritage Impact Assessment

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## 4.5.6 Roofs on new buildings:

*Roofs are an important visual feature in the district. The district is characterized by a variety of roof forms: front gable, side gable, cross gable, hip and truncated hip. Mansard and gambrel roofs are not commonly found on historic buildings in the district; therefore, these roof forms should be avoided. Flat or shallow pitch roofs should also be avoided in new construction.*

*The majority of the buildings now have asphalt shingle roofs. Wood or asphalt shingles would be appropriate for new construction within the district.*

## 4.5.7 Composition:

*The buildings are characterized by a tripartite division of the main elevation: foundation, wall and roof.*

*A shallow foundation is frequently differentiated from the wall above by construction in stone. The plinth, to carry water away from the wall, provides a horizontal break between the foundation and the wall.*

*Above the foundation, the main entrance is generally located on the street elevation, above grade and connected by steps and path to the sidewalk. The entrance on the ground floor is balanced, either symmetrically or asymmetrically, by window openings. The second floor, when present, is characterized by a flow of windows across the wall.*

*The wall is set apart from the roof by the strong horizontal line of the eaves. The roof mass is often punctuated with dormers and chimneys.*

*New residential buildings should maintain the tripartite division of foundation, wall and roof and respect the typical architectural elements of the main facades.*

## 4.5.8 Windows and entrances on new buildings:

*Generally window openings are vertical format and rectangular. There are also numerous examples of segmental and round headed openings.*

*New window designs are encouraged that generally reflect vertical and rectangular dimensions. On facades that face the street, windows should maintain proportions of neighbouring properties. Large, full-length, multi-storey or picture windows are best avoided.*

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*Entrances are usually an important element of the principal elevation, frequently highlighted with architectural detailing such as door surrounds and porches and recessed or projected from the wall face for emphasis. Doors are solid or partially glazed, single or half-leaf double doors. Avoid full size double doors and large amounts of glazing. Maintain the importance of the entrance way on the main elevation.*

## *4.5.9 Exterior cladding: materials and colours:*

*Brick veneer, stucco cladding and a combination of these two materials are the most prevalent wall materials in the district. Wooden cladding is also represented, often in combination with stucco. There is one stone structure covered in stucco in the district. Wall materials of new construction should reflect the predominant traditional materials and their respective colours: brick (red) and stucco (light). Wood (clapboard) is also considered to be an acceptable walling material. Wooden shingles may also be considered acceptable when used on upper wall surfaces with stucco or as decorative features. Use of concrete or other masonry blocks, metal, synthetic sidings should be avoided.*

*Windows and doors in the area are predominantly painted wood. Avoid synthetic or metal clad window and door units and untreated or natural wood.*

*Stone foundations are appropriate for new construction; however, carefully selected and laid textured concrete or masonry blocks can provide a more economical alternative. Avoid using materials that were primarily utilized for wall construction such as bricks and do not parge or stucco the foundation wall.*

## 6.1.4 –Guidelines for Landscape treatment is outlined in Section 5.0:

### *5.4.2 Recommended locations for tree planting:*

*The priority areas for street tree planting are in locations where there is currently no tree canopy. New street tree planting should be undertaken where the size of the boulevard and the location of underground services permit*

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

**Table 1: Recommended locations for tree plantings**

<b>1) Areas with no tree canopy</b>	
<b>Street</b>	<b>Location</b>
Trafalgar Road	From the north district boundary south to Lawson Street and from Sumner Avenue to Church Street.
MacDonald Road	From Trafalgar Road to Reynolds Street
<b>2) Areas with intermittent tree canopy</b>	
Spruce Street	From Trafalgar Road to Reynolds Street
Reynolds Street	From Lawson Street to Palmer Avenue
Allan Street	From Palmer Avenue to Randall Street

### 5.4.3 Boulevards:

*The grass boulevard running between the edge of the road, curb, and the public sidewalk is found on many streets in the neighbourhood. This green strip provides room for the planting of street trees and contributes significantly to the pedestrian environment. Where these boulevards exist in the neighbourhood they should be preserved.*

*This strip, where possible, should contain shrubs, turf and street trees in order to provide visual interest to the pedestrian environment.*

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## 6.2 Heritage Plan Conformity Assessment – “Non-Heritage”

The TRHCD Plan requires any work on properties to be in keeping with the Heritage character of the District.

### 6.2.1 Existing Building:

The existing medical office was built after 1952, and is considered “Non-Heritage” by the TRHCD. The building is of a volume, style, use and site plan layout that is not in keeping with the Heritage character of the District. Given the re-location of the hospital, it has lost any contextual relationship to the former hospital campus. As such, it has no heritage value and can be demolished

### 6.2.2 Propose Infill Townhouse development:

The design of the infill townhouse development at 358 Reynolds St. has been undertaken with consideration given to the Guidelines listed in the TRHCD Plan. The Property Owners have done their best to balance the design solution with requirements of the building program, the TRHCD guidelines and with those of other Authorities having jurisdiction.

The proposed design has been generated following the TRHCD design guidelines:

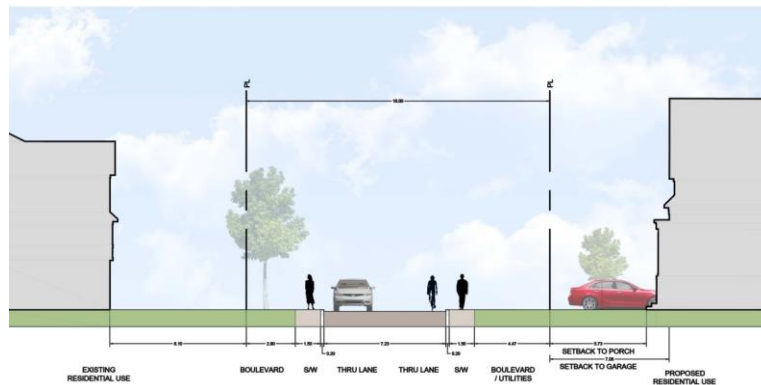
- 1) Subsection 4.5.2 New Building Height: The building height is proposed at 3 storeys. Mitigation strategies to reduce the impact of the building height include:
  - i. the proposed building height is similar as to the existing building on site and with sloped roofs not as visually dominating;
  - ii. a truncated hip roof design with a strong eaves line at the second floor to visually reduce the building height;
  - iii. hipped side roof slopes to visually reduce the volume of the roof;
  - iv. generous rear yard setbacks to keep the rear wall of the townhouse blocks away from adjoining neighbouring home;
  - v. undulation within the facade depth further accentuates the vertical rhythm;
  - vi. respect of a 45 degree angular plan from the ridge of neighbouring homes.

# Heritage Impact Assessment

## Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

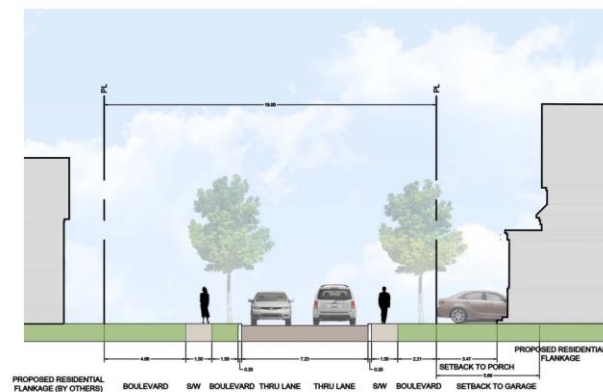
SECTION 1 - MACDONALD ROAD

NAK  
design strategies



SECTION 2 - REYNOLDS STREET

NAK  
design strategies



- 2) Subsection 4.5.3 Width:
  - i. the interior side yard of the proposed townhouse block matches that of its neighbour;
  - ii. the site plan layout proposes two townhouse blocks with interior side yard setbacks totaling 2.80m between them in keeping with average site yard setbacks.
- 3) Subsection 4.5.4 Proportion:
  - i. the pairing of the units with the second storey eaves line punctured by gable bays creates a vertical visual proportion in keeping with the neighbourhood character and building rhythm.
- 4) Subsection 4.5.5 Relationship to the Street:
  - i. the townhouse blocks are set within general neighbourhood setback distances to the street and are in general keeping with the building fronts of their adjoining neighbours;
  - ii. garages are set behind the front porches and projecting bay windows.

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- iii. porches have been designed to accentuate the front door so as to emphasize their predominance in the elevation, in keeping with the guidelines;
- iv. entries face all major streets.



- 5) Subsection 4.5.6 Roof on new buildings:
  - i. the roof shape is hipped with a truncated hip front portion in order to reduce its visual height;
  - ii. a strong eaves line further visually reduces the building height.
  - iii. front to rear roof slopes of 6:12 and side slopes are 14:12 are proposed;
  - iv. gable bays penetrate the eaves line and dormers dot the roof.
  - v. asphalt shingles are proposed.
- 6) Subsection 4.5.7 Composition:
  - i. the elevations use the garages to create a visual base;
  - ii. the front entries are at grade with walkways connecting them to the public sidewalk and covered with tall roof to accentuate their prominence;
  - iii. columns, soldier coursing and transoms further articulate the entry.
  - iv. entries are paired where possible to create a rhythm balance by the paired bays;
  - v. the wall is set apart from the roof by a strong eaves line;
  - vi. the roof is punctuated by dormers and gable ends;
  - vii. an architecturally enhanced elevation with entry is proposed facing Reynolds St.

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- 7) Section 4.5.8 Windows and entrances on new buildings:
  - i. the proportion of more wall than window has been followed;
  - ii. general window openings are more vertical than horizontal;
  - iii. segmented transoms in windows have been used;
  - iv. full length multi-storey windows have not been used on facades facing the street;
  - v. entrances have been accentuated with projecting tall porches, single, partially glazed doors with transoms, side lites, brick detailing;
  - vi. entries face all major streets.
  
- 8) Section 4.5.9 Exterior cladding: materials and colours:
  - i. masonry cladding is proposed;
  - ii. a bi-chromatic brick colour scheme is proposed;
  - iii. a predominantly dark (red) brick is proposed with buff accent bricks to match prevalent material and colours in the district;
  - iv. the use of concrete, masonry blocks and metal and synthetic sidings has not been used;
  - v. energy efficient windows will be made of vinyl with thick frames and sashes to give the appearance of wood.
  - vi. modern materials are proposed and will have the visual appearance of traditional materials.
  - vii. there are no shutters on the existing buildings and none are proposed.
  
- 9) Section 5.4.2 Recommended Locations for plantings:
  - i. street trees are proposed along MacDonald Rd and Reynolds Rd as permitted by the Town;
  - ii. street trees will be selected from the list in Section 5.5, Table 2.
  
- 10) Section 5.4.3 Boulevards:
  - i. boulevards will have grass planted wherever possible;
  
- 11) Section 5.4.6 Front yard parking:
  - i. driveways will be kept as narrow as possible;
  - ii. driveways will be treated in an upgraded surface treatment
  - iii. cut cuts will be kept as narrow as possible.

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

## 7.0 Summary Statements and Recommendations

### 7.1 The Infill Townhouse Development at 358 Reynolds Street:

The property at 358 Reynolds Street is located within the Trafalgar Road Heritage Conservation District.

The existing building is considered Non-Heritage by the TRHCD plan and has no Heritage value. Its building can be demolished.

The proposed infill townhouse development at 358 Reynolds Street has been undertaken with consideration to the Guidelines listed in the TRHCD Plan and where non-compliant proposes mitigating design solution to minimize the impact on the local heritage character.

The proposed infill townhouse development at 358 Reynolds Street is proposed to be executed the TRHCD Plan Guidelines in the following manner:

- 1) the office use of property is to be replaced with residential use – more in keeping with the neighbourhood;
- 2) the building height is proposed at 3 storeys. Mitigation strategies to reduce the impact of the building height include:
  - i. the proposed building height is similar as to the existing building on site and with sloped roofs not as visually dominating;
  - ii. a truncated hip roof design with a strong eaves line at the second floor to visually reduce the building height;
  - iii. hipped side roof slopes to visually reduce the volume of the roof;
  - iv. generous rear yard setbacks to keep the rear wall of the townhouse blocks away from adjoining neighbouring home;
  - v. undulation within the facade depth further accentuates the vertical rhythm;
  - vi. respect of a 45 degree angular plan from the ridge of neighbouring homes.
- 3) the interior side yard of the proposed townhouse block:
  - i. matches that of its neighbour;
  - ii. the site plan layout proposes two townhouse blocks with interior side yard setbacks totaling 2.80m between them in keeping with average site yard setbacks.
- 4) With respect to proportions, the pairing of the units with the second storey eaves line punctured by gable bays creates a vertical visual proportion in keeping with the neighbourhood character and building rhythm.



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- 5) With respect to the relationship to the street:
  - i. the townhouse blocks are set within general neighbourhood setback distances to the street and are in general keeping with the building fronts of their adjoining neighbours;
  - ii. garages are set behind the front porches and projecting bay windows.
  - iii. porches have been designed to accentuate the front door so as to emphasize their predominance in the elevation, in keeping with the guidelines;
  - iv. entries face all major streets.
  
- 6) with respect to the roof:
  - i. the roof shape is hipped with a truncated hip front portion in order to reduce its visual height;
  - ii. a strong eaves line further visually reduces the building height.
  - iii. front to rear roof slope is 6:12 and side slopes are 14:12;
  - iv. gable bays penetrate the eaves line and dormers dot the roof.
  - v. asphalt shingles are proposed.
  
- 7) with respect to the composition:
  - i. the elevations use the garages to create a visual base;
  - ii. the front entries are at grade with walkways connecting them to the public sidewalk and covered with tall roof to accentuate their prominence;
  - iii. columns, soldier coursing and transoms further articulate the entry.
  - iv. entries are paired where possible to create a rhythm balance by the paired bays;
  - v. the wall is set apart from the roof by a strong eaves line;
  - vi. the roof is punctuated by dormers and gable ends;
  - vii. an architecturally enhanced elevation with entry is proposed facing Reynolds St.
  
- 8) with respect to windows and entrances:
  - i. the proportion of more wall than window has been followed;
  - ii. general window openings are more vertical than horizontal;
  - iii. segmented transoms in windows have been used;
  - iv. full length multi-storey windows have not been used on facades facing the street;
  - v. entrances have been accentuated with projecting tall porches, single, partially glazed doors with transoms, side lites, brick detailing;
  - vi. entries face all major streets.
  
- 9) With respect to exterior cladding, material and cladding:
  - i. masonry cladding is proposed;
  - ii. a bi-chromatic brick colour scheme is proposed;

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

- iii. a predominantly dark (red) brick is proposed with buff accent bricks to match prevalent material and colours in the district;
  - iv. the use of concrete, masonry blocks and metal and synthetic sidings has not been used;
  - v. energy efficient windows will be made of vinyl with thick frames and sashes to give the appearance of wood.
  - vi. modern materials are proposed and will have the visual appearance of traditional materials.
  - vii. there are no shutters on the existing buildings and none are proposed.
- 10) with respect to street tree planting:
- i. street trees are proposed along MacDonald Rd and Reynolds Rd as permitted by the Town;
  - ii. street trees will be selected from the list in Section 5.5, Table 2.
- 11) the with respect to boulevards, boulevards will have grass planted wherever possible.
- 12) with respect to front yard parking:
- i. driveways will be kept as narrow as possible;
  - ii. driveways will be treated in an upgraded surface treatment
  - iii. cut cuts will be kept as narrow as possible.

The above components of the proposed design solution illustrate the attention to the TRHCD Plan Guidelines paid by the Property Owner and their Architects. They have not only created a project which satisfies the Client's spatial needs but also successfully fits the proposed infill townhouse development into the Trafalgar Road HCD.

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

## 8.0 Mandatory Recommendations:

### 8.1 Mandatory Recommendation regarding the Infill Townhouse development at 358 Reynolds Street in the Trafalgar Road Heritage Conservation District:

With respect to the proposed Infill Townhouse Development at 358 Reynolds Street, it is recommended that:

- 1) The existing Building at 358 Reynolds St is considered “Non-Heritage”; has no Heritage value and can be demolished;
- 2) The Infill Residential Townhouse development is in general compliance with the Principals, Objectives and Guidelines of the Trafalgar Road Heritage Conservation District Plan;
- 3) The Infill Residential Townhouse development does not negatively impact the Heritage character of the Trafalgar Road Heritage Conservation District; and
- 4) The Infill Residential Townhouse development be approved.

## 9.0 Authourship

Report Prepared By:

VINCENT J. SANTAMAURA, ARCHITECT INC.

**Date:**

08 December 2023



Vincent J. Santamaura, B. Arch, OAA, MRAIC, CaBGC, CAHP  
Principal Architect

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

## Appendix 1: Response to Town Pre-Consultation Comments, dated March 29, 2023

- o Staff comments-Building Height: The proposed buildings do not maintain nor respect the dominant building height of the district.
- Response:
  - 3 storey buildings exist in the district;
  - The project replaces an existing 4 storey building with a 3 storey building with its visual impact mitigated by hip truncated roof design, and
  - side sloping hipped roof design on the sides to be more in keeping with the neighbourhood character and built form.
- o Staff comments -Width: The proposed buildings do not maintain the building width and side yard spaces of the district streetscape.
- Response:
  - There are a variety of side yard setbacks with in the district;
  - The side yards match or exceed those of the neighbouring lots.
- o Staff comments - Proportion: The proposed buildings have individual units that are oriented vertically, but the overall buildings do not maintain the proportions of neighbouring properties.
- Response
  - The elevations have been re-designed to emphasize the vertical proportions of the units, but with a lower eaves line, create visual proportions and rhythm to match that of the neighbourhood properties.
- o Staff comments - Relationship to the street: The site plan does not provide enough context to see the building's setback as it compares to neighbouring properties. The garages are far too prominent along the streetscape and are inappropriate given their setbacks and the amount of garages included within the design.
- Response
  - A context plan showing setbacks has been provided;
  - setbacks are in keeping with the range in the neighbourhood;
  - the buildings do not project too much in front of their neighbours;
  - porches have been pulled forward and made taller to be more prominent than the garages.
- o Staff comments - Roofs: The proposed steep mansard-style roof of the building is not compatible with the district. The roof is far too large and prominent and is overwhelming on the heritage streetscape, and does not respect the overall character of the district.
- Response
  - the roof has been re-designed as a truncated hip with a front to rear slope of 6:12 and side slope of 14:12;
  - The roof mass been broken up with gabled bays and dormers.
- o Staff comments - Composition: The foundation is too high, with the entrance too far above ground level, the balconies and balcony doors are not appropriate on the front elevation, and the roof mass is far too large and expansive.

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

- Response
  - the entries have been dropped to grade;
  - balconies and balcony doors have been removed;
  - the roof mass has been broken up with gabled bays and roof dormers.
  
- o Staff comments - Windows and entrances: The entranceway is overshadowed by the garages which are too prominent and protrude in front of the entrance.
  
- Response
  - the entries have been dropped to grade;
  - porches have been pulled forward and made taller to be more prominent than the garages.
  
- o Staff comments - Exterior cladding: The drawings do not include detailed materials, but overall the proposal seems to include a lot of heavy masonry material that creates a heavy, dominating look that is overwhelming in the district. Stone and pre-cast materials are not appropriate in the district when used in such large amounts.
  
- Response
  - a material package has been provided;
  - stone and precast have been removed except for window sills,
  - masonry detailing has been proposed for window headers;
  - red brick has been proposed for the dominant cladding material with buff brick accents which inspired by neighbourhood colour palettes;
  - porches have been re-designed to be more open with lighter looking wood columns.

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

## Appendix 2: Biography of Authour:

### SELECTED PROFESSIONAL RESUME

**Vincent J. Santamaura**, B. Arch, MOAA, MRAIC

#### Overview:

Vincent has evolved his over to 35 years of experience in the Construction Industry from Vincent J. Santamaura, Architect into a founding partner of SRN Architects Inc. A creative designer, familiar with a variety of Building systems, and the Building and Approvals process, he applies his knowledge to solving the building needs of his clients.

Trained and registered as an Architect, Vincent graduated from the University of Toronto. He has been active in the Greater Toronto Area – from downtown infill housing to new communities to historic renovations, adaptive re-use to high-rise. Vincent has worked for award winning architectural firms and has run his own practice. He has worked for a large land developer/home builder as Staff Architect and Community Planner where he was responsible for designing new communities, lotting modules and commercial and residential unit forms. Fully versed in the grand picture, Vincent applies his knowledge and experience back into the urban and architectural design fields.

Familiar with a variety of building systems, Vincent is comfortable designing in steel frame, cast-in place concrete or wood or light gauge steel framing. His design solutions balance urban concerns, client needs, and budget demands. Sustainability has always been an interest of Vincent's since his university days having been involved in passive energy design since the first oil crisis, and this has led to an interest in building envelope systems and an exploration of the new techniques. Fundamentally, though, it is the satisfaction of the client's needs that drives the building design solution and the delivery of it on time and on budget.

Vincent derives a large amount of his design inspiration from our Ontario Heritage. He's been the Chair of the Uxbridge LACAC and has been active in the preservation efforts of the Foster Memorial and the Lucy Maud Montgomery House, both in Leaskdale. He designed the York/Durham Heritage Railway/Go Train Station in Stouffville, and renovations to the Goodwood Town Hall (1875) and the Uxbridge Music Hall (1901). With these works, Vincent has developed strong interpersonal skills interacting with various communities, committees and municipal governments. This sensitivity to the existing built (and social) environment ensures that any design intervention will respect its neighbours.

Keenly aware of the complex issues and interests in building communities, Vincent uses his design skills, his consensus building skills and his experience to arrive at a balanced solution to any design challenge.

#### Education:

- 2001 to present – OAA Professional Development Continuing Education Course Certificates
- 1986 to 1989 – AutoCAD Certificate courses, Ontario CADD/CAM Centre, Cambridge, Ontario
- 1985 – Construction Specifications Course, CSC, Toronto
- 1983 – Bachelor of Architecture, University of Toronto

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

## Professional Memberships:

2012 to 2017 – Member, Alberta Association of Architects  
2010 to present – Member, Building Specialist, Canadian Association of Heritage Professionals  
1981 to present – Member, Ontario Association of Architects, Registered 1989  
1983 to present – Member, Royal Architectural Institute of Canada

## Community Memberships:

2015 to 2018 – Member, Heritage Whitby/LACAC, Town of Whitby  
2009 to 2012 - involved in Local Architectural Conservation Advisory Committee to the Town of Whitby  
1993 to 1996 – Member/Chairman, Heritage Uxbridge/LACAC, Town of Uxbridge  
- involved in Local Architectural Conservation Advisory Committee to the Town of Uxbridge  
1993 to 2002 – Member, Friends of the Foster Memorial, Town of Uxbridge  
- involved in the fund raising, preservation and designation efforts for the Foster Memorial in the Town of Uxbridge  
1994 to 2002 – Member, York/Durham Heritage Railway Association, Stouffville  
- involved the running of the heritage railway between Stouffville and Uxbridge  
1995 to 1998 – Member, Celebration of the Arts Committee, Town of Uxbridge  
- involved in organizing the annual Cultural Celebration in the Town of Uxbridge

## Professional Activities and Selected Projects:

### Expert Witness – Heritage Matters:

**Expert Witness** – Architecture/Site Planning/Urban Design: Ontario Municipal Board -  
Recognized as an **Expert** in Architecture, Site Planning and Urban Design by the Ontario Municipal Board for Testimony during Heathwood Homes Appeal of the City of Toronto By-Law no. 2017-62.

**Expert Witness** – Heritage Matters: Ontario Municipal Board -  
Recognized as an **Expert** in Heritage Matters by the Ontario Municipal Board for Testimony during Dunbar Homes Appeal of the City of Mississauga Refusal to Enact By-Law no. 0225-2007

**Expert Witness** – Heritage Matters: Ontario Municipal Board  
Recognized as an **Expert** in Heritage Matters by the Ontario Municipal Board for Testimony during Testimony for Vitmont Holding Inc Appeal of the Town of Aurora Non-Decision on Site Plan for 15160 Yonge Street & No. 5 Tyler Street, Aurora

**Expert Witness** – Heritage Matters: Ontario Municipal Board -  
Recognized as an **Expert** in Heritage Matters by the Ontario Municipal Board for Testimony during Ballantry Homes Appeal of the Town of Markham By-Law no. 2006-78

### Heritage Impact Statements/Reports:



**Heritage Impact Statement/Preservation Plan – 68 Daisy Street, City of Toronto (Etobicoke):**  
**Architect/Heritage Architect** who analyzed and authored a Heritage Impact Assessment and

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

Preservation Plan for the Vincent Massey Public School on the impact of a new cluster of townhouses proposed around it in the City of Toronto.

## Heritage Impact Statement – 4583, 4589 & 4601 Mississauga Road, City of Mississauga:

Analyzed and authored a Heritage Impact Statement for the impact of a new cluster of homes on the Credit River Cultural Landscape (Heritage Registered Inventory) and the Mississauga Road Scenic Route (Heritage Register Inventory) in the City of Mississauga.

## Heritage Impact Assessment - 6 Mann Street, Clarington (Bowmanville):

**Architect/Heritage Architect** who analyzed and authored a Heritage Impact Assessment for the impact of an infill project of three single detached homes in the Town of Clarington (Bowmanville).

## Heritage Impact Statement – 4390 Mississauga Road, City of Mississauga:

Analyzed and authored a Heritage Impact Statement for the impact of a proposed semi-detached and townhouse development on the Mississauga Road Scenic Route (Heritage Register Inventory) in the City of Mississauga.



## Cultural Heritage Impact Review Assessment/Preservation Plan – The Thomas Wright House and the McGillivray-Shore House

**8161 & 8177 Kipling Avenue, City of Vaughan:**  
**Architect/Heritage Architect** who analyzed and authored a Cultural Heritage Impact Statement and Preservation plan for the re-location and renovation of two Designated Homes and the impact of a proposed stacked townhouse project in the

Woodbridge Heritage Conservation District, in the City of Vaughan. **(2017 Vaughan Urban Design Award Winner)**

**Heritage Impact Statement – 10056 & 10068 Keele Street, City of Vaughan:**  
**Architect/Heritage Architect** who analyzed and authored a Cultural Heritage Impact Statement for a proposed townhouse project in the Maple Heritage Conservation District, in the City of Vaughan.

## Heritage Impact Statement/Preservation Plan - Stiver Tenant House

**9721 Kennedy Road, City of Markham**  
**Architect/Heritage Architect** who analyzed and authored a Cultural Heritage Impact Statement Preservation plan for the re-location, renovation, and addition of a Designated Stiver Tenant Home project in the Town of Markham. (on-going)



## Cultural Heritage Impact Review Assessment/Preservation Plan – The Thomas Watson House

**8934 Huntington Road, City of Vaughan:**  
**Architect/Heritage Architect** who analyzed and authored a Cultural Heritage Impact Statement and Preservation plan for the renovation of a formerly Designated Home as part of the proposed Arlington Estate Banquet Hall re-development project in the City of Vaughan.

**Heritage Impact Assessment/Preservation Plan – 6 & 12 Durham Street , Town of Whitby:**  
**Architect/Heritage Architect** Analyzed and authored a Heritage Impact Assessment and prepared a Preservation plan for the re-location,



# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

renovation, and addition of two homes of Heritage value as part of “Olde Whitby-The Mews” redevelopment project in the Town of Whitby (Brooklin).

## Restoration:



### The Music Hall (1901), Uxbridge:

Architect for the renovation of the cultural centre of the thriving artistic life of Uxbridge since 1901. The facilities of the Historically Designated Music Hall were updated, and the stage was restored.

## Adaptive Re-Use:



### 11 Woodlawn Avenue, Toronto:

**Project Architect** for a conversion of a church building into condominium suites. The existing 4,000 sf building shell had another 8,000 sf of building area inserted into its envelope to create six luxury 2-storey units.

## Mixed Use Projects:



### Old Brooklin-The Mews, Brooklin:

**Architect/Heritage Architect** for the infill and extension of downtown Brooklin’s Heritage District’s main street with a mixed-use project using traditional living above retail programming and local heritage architectural styles.

## Institutional:



### Go Transit Stouffville and York Durham Heritage Railway Terminus, Stouffville:

**Architect** for the Heritage inspired Go Transit Station Stouffville which also acts as a terminus station for the York Durham Heritage Railway line between Stouffville and Uxbridge.

# Heritage Impact Assessment

Proposed Residential Development, 358 Reynolds Street, Oakville Ontario

## Commercial:



### 10 Richmond Street, Maple:

**Architect** for a commercial infill building in Downtown Maple's Heritage, currently approved by the City of Vaughan Heritage Committee, the Maple Streetscape Committee and City of Vaughan Council.

### Architectural Control Guidelines:

#### Spring Creek, Waterdown:

**Control Architect** who created Architectural Control Guidelines which selected all components for the accurate re-creation of Georgian, Edwardian, Queen Anne, Arts & Crafts and Canadiana styles in this heritage inspired residential community.

#### Kleinburg Estates, Vaughan (Kleinburg):

**Control Architect** who created Architectural Control Guidelines which selected all components for the accurate re-creation of Georgian, Queen Anne, Second Empire and Victorian styles in this residential community in the Heritage Conservation District of Kleinburg.

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**End of Report**